



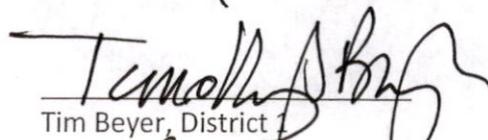
Boone County Policy on Speed Humps

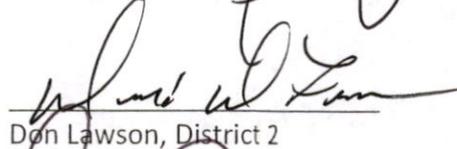
Prepared by: Jeff Hill
Engineer, A&F Engineering
January 28, 2026

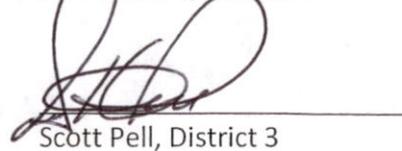
Reviewed by: Nicholas Braner
Project Manager
February 18, 2026

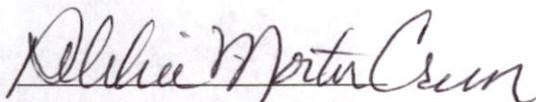
Recommended by: Nick Parr
Director of Administration
March 4, 2026

Adopted by: Boone County Board of Commissioners


Tim Beyer, District 1


Don Lawson, District 2


Scott Pell, District 3

Attest: 
Debbie Morton-Crum, Auditor

Date: 03/09/2026

Boone County Policy on Speed Humps

TABLE OF CONTENTS

Background/Reason for Report	3
Goals	4
Definition of a Speed Hump	4
Summary of Findings	4
Steps to Gain Installation	5
Speed Hump Policy	6
Guidelines for Consideration/Installation	6
Other Considerations	8
Design and Placement of Speed Humps	8
Construction of Speed Humps	11
Removal of Speed Humps	12

APPENDICES

- Appendix A: Petition for Speed Hump Installation
- Appendix B: Speed Hump Installation Investigation Form
- Appendix C: Sample Letter from Police/Fire Department & Schools
- Appendix D: Agreement to Install Speed Humps
- Appendix E: Speed Hump Information Brochure

Boone County Policy on Speed Humps

Background/Reason for Report

Growing amounts of vehicular traffic due to increased population, changes in neighborhood lot sizes sometimes creating increased density, and under-designed Arterial-type roadways whose capacity has been reached has led to the diversion of vehicular traffic into residential areas. Additionally, there is increasing disregard for signed regulatory and warning restrictions such as speed limits, stop and yield signs, and other traffic control elements. Drivers' tolerance, driver distraction, and voluntary compliance with these traffic control devices has resulted in generally increased safety concerns. Increased congestion on arterial and collector roadways resulting in higher traffic volumes and increased speeds on local streets has prompted concern from Boone County officials and residents.

The following are some of the commonly experienced problems and neighborhood traffic concerns:

- **Traffic Safety**: The occurrence of vehicular crashes or the fear of crashes (near-miss) is a common concern of residents.
- **Vehicular Speeds**: Excessive and/or increased speeds on local roads and streets is a frequent complaint from Boone County residents.
- **Traffic Volumes**: Total amounts of vehicular traffic and general increases in traffic volumes in residential communities are a frequent cause of complaints.
- **Traffic Composition**: Often citizens are concerned with types of vehicles. Trucks, school buses, trash vehicles, delivery vehicles, and construction traffic are considered to be greater hazards than passenger vehicles.
- **Existing Geometrics**: Existing neighborhood streets may have been constructed in a manner that limits the ability for existing curvature or geometry to help control speeds which encourages non-compliance with posted speed limits.

Based on these concerns vocalized by the residents of Boone County, coupled with the demand placed on the County's arterial road system, plus the on-going need to maintain the quality of life that Boone County residents demand, it is this policy that establishes and encourages improved traffic control via development of this **speed hump policy** to help control and calm traffic in neighborhoods.



Further, there is national guidance and support for the use and deployment of many different traffic calming tools to help control speeds and improve safety. Speed humps are one of many tools identified by the Federal Highway Administration (FHWA), the Institute of Transportation Engineers (ITE), the National Association of City Transportation Officials (NACTO), and other industry-expert groups as a proven traffic calming device.

This policy sets forth a process for the potential implementation of speed humps and the evaluation and analysis for needed to consider installation. Speed humps should be installed only after a traffic survey, site-specific evaluation, and analysis has been completed under the direction of the County Highway Department, or its authorized representative. Installation should be considered after other measures, including but not limited to, increased signing, police enforcement, and other methods that have been implemented without measurable success.

Goals

Boone County has studied many available devices that could be used to calm traffic in neighborhoods and has determined that the installation of speed humps as a retrofitted traffic calming measure is anticipated to be an effective solution which may be installed in a reasonably timely manner at a moderate cost. This policy has narrowed its vision and focused its scope upon the installation of speed humps in Boone County.

This policy is for the retrofitting of existing street sections. The Highway Department will continue to work with developers during the preliminary design phase of new subdivisions to ensure quality street design resulting in seeking to avoid the need for these or other similar traffic calming devices.

Definition of a Speed Hump

A speed hump is a raised area in the pavement surface extending across the roadway. Speed humps are roadway geometric design features that create a gentle vehicle rocking motion that causes most vehicles to slow to approximately 15 miles per hour (MPH) or less at each hump, seeking to maintain a speed of approximately 25 MPH to 30 MPH between properly spaced humps. Speed humps should not be confused with speed bumps which cause discomfort to motorists and a shock to vehicles only at low speeds.

Further, they should also not be confused with other traffic calming devices like speed tables which are raised sections of roadway pavement that are often-times longer than typical speed humps and sometimes carrying through an entire intersection.



Summary of Findings

Boone County through these actions is adopting a Countywide policy for the potential deployment of speed humps. This policy would satisfy residents of neighborhoods where there is a demonstrated speed problem. In addition, this policy would seek to reduce or eliminate vehicular crashes on local streets where there is a demonstrated crash problem. Although the installation of speed humps in a

subdivision may cause an inconvenience to some existing neighborhood residents as well as cut-through motorists, the policy would benefit the County as a whole.

Steps to Gain Installation

Speed humps will be installed only in those neighborhoods who have demonstrated their preference for them, and the Highway Department has studied and evaluated the appropriateness for installation as discussed in this policy. A summary of these steps are here, with greater details and forms found in the policy and appendices:

- Prior to any other action being taken, the petition form shown in Appendix A will need to be signed by seventy-five percent (75%) of the homeowners in the subdivision, with verification subject to tax records at the Office of the Boone County Auditor.
- The Department will require support by the appropriate fire department, law enforcement agency, and school corporation—all based upon specific location in Boone County—on the approved County form letters, or very similar, shown in Appendix C.
- The *Agreement to Install Speed Humps*, as shown in Appendix D, will need to be completed and executed.
- The anticipated total cost of the materials and the labor for the speed humps, warning signs, and pavement markings will be paid by the Homeowner's Association. The construction of the speed humps installation will be a cost borne by the requesting neighborhood and not by the County. Boone County has many significant larger roadway needs throughout the County.

Boone County Highway will provide the administration of contracts required for construction of the speed humps, signs, and pavement markings, and will also provide the construction inspection for the work. After construction is complete, the Boone County Highway Department will maintain the speed humps installed as part of its typical highway maintenance program.

SPEED HUMP POLICY

Guidelines for Consideration/Installation

Speed hump installations should only be installed where there is a documented and demonstrated traffic control need that can be improved by their installation. Their installation should be addressed only after other alternative traffic control elements that have reduced impacts (such as signs, markings, or other similar measures) have been considered and attempted.

An engineering study will be completed at the location/neighborhood requested. The study will help determine if speed humps are warranted, but also to assess the effect upon adjacent street facilities. Speed humps may cause traffic to divert and seek alternate routes. An estimate of the traffic diversion should be made so that the impact of the speed humps can be fully understood. If the installation will cause larger impacts to the adjacent roadway network, then they should not be considered. This study should be completed by the Boone County Highway Department, or their authorized representative. This study is an effort over and above the day-to-day operations and responsibility of the Highway Department and the cost of this study shall be reimbursed by the neighborhood or homeowner's association (HOA).

Speed humps will be installed only on local streets, as defined by Boone County Code or Boone County Thoroughfare Plan (e.g. residential subdivision streets). They will only be installed on streets with 2 travel lanes (20-ft. minimum pavement width and 40-ft. maximum width) with grades of no greater than 5 percent. If possible, the humps will be located on tangent sections of roadway and ideally not on severe vertical or horizontal curves. Speed humps will only be used where there is a minimum safe stopping sight distance as prescribed by the American Association of State Highway and Transportation Officials (AASHTO) Manual *A Policy on Geometric Design of Highway Streets*. Speed humps should not be installed near intersecting streets.

When speed humps are being considered to address speeding concerns, studies will be performed to confirm the magnitude and extent of the speeding problem to ensure that speed humps can be expected to solve it. An investigation will be completed to gather data for speed hump evaluation, including information such as:

- **Traffic Speed:** speed humps will only be installed on streets where the speed limit is posted, with supporting ordinance, at a speed limit that is 30 MPH or less.
- **Traffic Volume:** the humps will only be installed where the average daily traffic (ADT) is less than 3,000 vehicles per day (VPD), but also on streets where ADT is greater than 500 VPD. When speed humps are being considered to address higher traffic volume locations, their use will receive special evaluation and justification prior to approval.
- **Significant Speed:** consideration for installation will be given greater significance if the measured 85th-percentile speed(s) on a street exceeds the posted speed limit by more than 15 MPH.
- **Crashes:** investigation of crash volume and type on the representative street(s) or neighborhood will be completed to investigate if the installation of speed humps can be expected to reduce crashes.
- **Pedestrians/Schools:** proximity to schools and the number of pedestrians will be gathered for evaluation.

This type of site-specific information will be gathered to investigate the need for speed humps. A sample investigation form is included in Appendix B.

With the adoption of this policy, it is currently unknown what the public interest will be for the potential consideration and installation of speed humps. The policy establishes a point system to establish minimum criteria to be met for installation but will also be used for prioritization for the Highway Department should the number of requests for speed humps be significant. It is anticipated that a contract for their installation could be bid during each summer construction season, if needed. This will be governed by a point system detailed in the following paragraphs. This system will consider the following: traffic volumes, crash history, vehicular speeds, and proximity to schools or other pedestrian facilities:

- **Average Daily Traffic**

The total number of vehicles traveling on the roadway where speed humps are desired will be a basis for points assignment. Vehicular volume will be obtained by traffic counts taken by the Boone County Highway Department or their authorized representative. A point value is obtained by dividing the total number of vehicles by 100. If the segment of road exceeds 1,000 feet, the average of multiple counts may be used.

Example:

$$1,520 \text{ total vehicles on the roadway } 1,520/100 = 15.2 \text{ points}$$

- **Speed**

The speed of vehicles traveling on the roadway where speed humps are desired will be another basis for points assignment. These vehicular speeds could be gathered during the traffic counts taken by the Boone County Highway Department. A point value is obtained by subtracting the posted speed limit from the 85th-percentile speed. If the segment of road exceeds 1,000 feet, the average of multiple counts will be used.

The 85th-percentile speed is the speed resulting from the previously mentioned traffic count; the value used will be the greatest 85th-percentile speed on the streets being studied for speed humps in an entire neighborhood.

Example:

37.2 MPH is average 85th-percentile speed. 25 MPH is posted speed limit.

$$37.2 - 25 = 12.2 \text{ points}$$

- **Crash History**

The vehicular crash totals over a minimum of 3 years will be tabulated for those crashes within the subdivision being investigated for speed humps will be an additional basis for point assignment.

The crash reports used are those reported to police agencies (Boone County Sheriff's Department, City of Lebanon Police Department, Town of Zionsville Police Department, etc.). Any subdivision location will receive four (4) points per reported crash per 1,000 feet on the street being considered for speed humps. Any crashes occurring at the intersection of an exiting road onto a major arterial or collector road will not be considered in this point assignment since speed humps internal to the neighborhood will not control speeds/crashes at a subdivision entrance.

Example:

2 crashes located on the road segment that is 1,200 feet long.

$$2 \times 4 \times \frac{1,000}{1,200} = 6.7 \text{ points}$$

- **Schools (Elementary & Middle)**

The number of elementary or middle schools located within an area of where speed humps are desired will be an additional basis for point assignment. The candidate subdivision will have three (3) points added to its total for each public or private elementary school or middle school that lies within a 1,000-foot radius of the candidate subdivision street.

- **Pedestrian Served Facilities**

The locations having pedestrian facilities near those subdivisions where speed humps are desired will be an additional basis for point assignment. If the subdivision street being considered for speed humps lies within a 500-foot radius of an individual pedestrian facility, such as a County or City park, neighborhood recreation area, church, shopping center or elderly housing, or a grouped pedestrian facility, three (3) points are received for each such facility (six points maximum).



This point system is comprised to establish warrants and a priority system for ranking potential candidate neighborhoods for the installation of speed humps. The minimum aggregate point total using the scoring system noted above to qualify for speed hump consideration is 25 points.

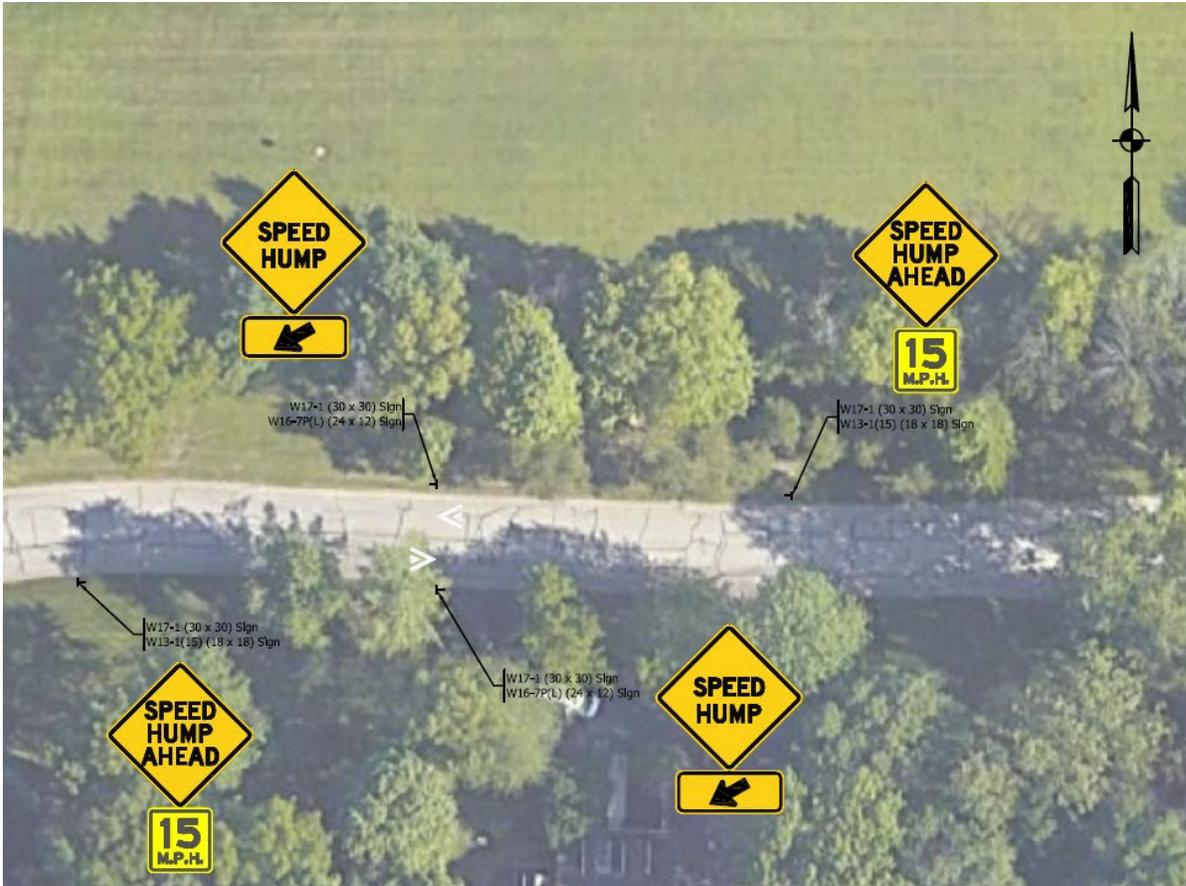
Other Considerations

According to the Manual on Uniform Traffic Control Devices (MUTCD), speed humps are traffic calming measures, but they are not traffic control devices. Speed humps are geometric design features of the roadway and should be designed, installed, operated, and maintained using accepted engineering principles and prudent engineering judgment.

Speed humps shall be installed with care and in a proper manner to maintain safe passage of roadways once complete. Local and state laws should be reviewed to identify any regulations pertaining to roadway design roadway maintenance, traffic control, or other elements that might be related to the use of speed humps or other geometric design features that may affect their installation. Careful consideration of the motoring public in general should be considered prior to enacting any installation.

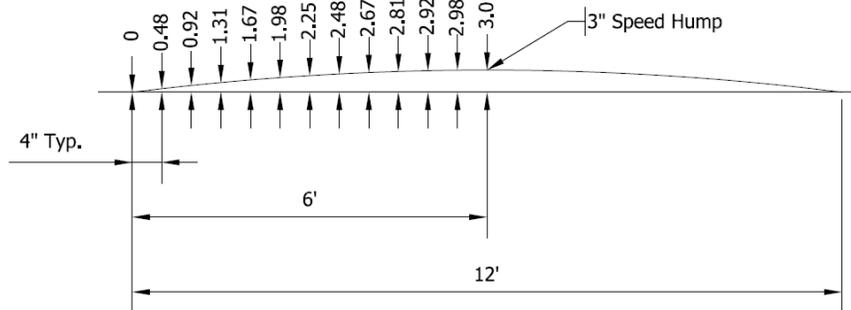
Design and Placement of Speed Humps

For the County's purposes of installing speed humps, it is desirable to use industry-accepted design and construction standards. Elements that should be studied and calculated are spacing, size, shape, and general configuration so they are an effective traffic calming tool. A typical depiction of speed humps, signing, and markings is shown on the following page, but special consideration should be given to any individual installation to make sure motorist safety is taken into account and other elements like drainage, utility impacts, contextual location, and other factors. Greater design details and elements are covered later in this policy.



Typical Speed Hump Layout

Boone County's criteria uses the circular/parabolic speed hump. The speed hump center height is 3 in. which will effectively calm traffic while having an acceptable height for school buses, emergency vehicles and other non-passenger vehicles. The figure below shows the typical parabolic dimensions for a 3-in. speed hump that is 12 ft. long along the roadway centerline.



Typical Speed Hump Cross Section

Typical traffic engineering practices would seek a range of speed hump spacing from approximately 200 ft. to 750 ft. between speed humps. This spacing is a suggested range, and the actual spacing will be determined by the County Highway Department or its authorized representative based on field observations and site-specific needs. The location(s) of speed humps will be selected to effectively control speed, but need to minimize the impact on roadway drainage, drainage structures, and utility

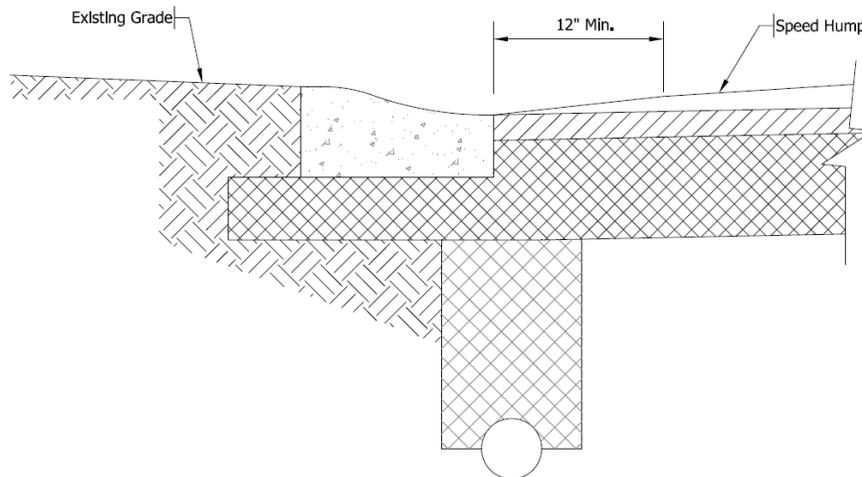
access locations. Speed humps should not be installed on blocks or cul-de-sacs shorter than 500 ft. in length. A guideline for spacing based on desired 85th-percentile speeds was established by a subcommittee of the California Traffic Devices Committee which is:

$$H_s = 0.5 [2(V_{85})^2 - 700]$$

where H_s = hump spacing (ft.)
 V_{85} = 85th percentile speed (MPH)

Utilizing the equation would realize a spacing of 250 feet and 550 feet for the 85th percentile speeds for 25 MPH and 30 MPH, respectively. Ideally, the spacing calculation will identify an approximate range for speed hump installation, but sound engineering judgement should be exercised while considering site specific conditions to develop actual spacing.

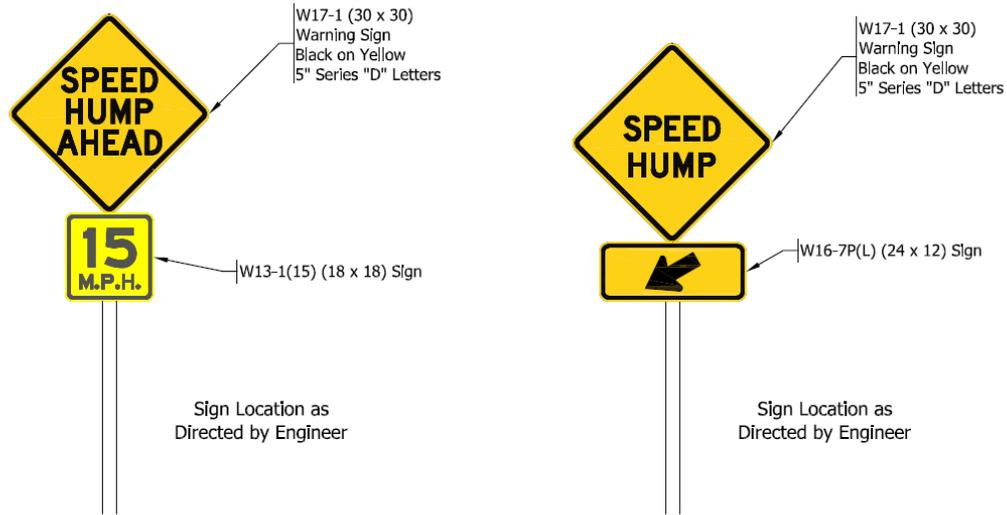
Where side ditches have been utilized for subdivision construction, the speed humps shall extend to the edge of pavement. Where streets have been constructed with a curb and gutter installation, the speed hump shall be tapered at each end a minimum of twelve inches as shown in the figure below.



Tapered Speed Hump at Curb and Gutter

Traffic control consisting of a combination of road signs, advanced warning signs, and pavement markings will be needed to warn motorists of the presence of speed humps. This control will also guide motorists on their actions on such a road.

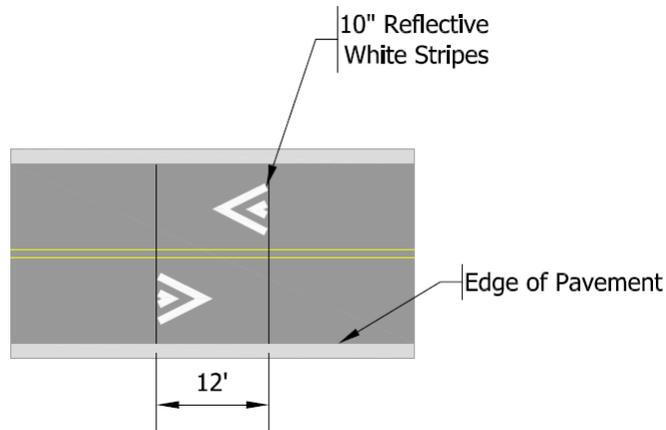
A 30" x 30" W17-1 warning sign (black letters on yellow sign) will be installed in advance of and at each speed hump facing each direction. This sign shall state the word "SPEED HUMP" and be a standard reflective sign in compliance with the Indiana MUTCD. An 18" x 18" W13-1 advisory speed plaque, made of similar materials as the warning sign, stating "15 MPH" will be installed below each warning sign. The sign installation will be placed on a standard U-channel post or a square channel post. The advanced warning sign should be placed at a distance so as to comply with the MUTCD based upon the posted speed limit. The typical installation is shown in the figure on the following page.



Warning Signs to be Located at Speed Humps

Pavement markings should be installed as shown in the figure below to supplement the installed warning signs. The pavement markings should be 10 in. wide reflective, white warning markings. The markings shall be hot-applied thermoplastic or preformed plastic and will aid in seeing the humps during inclement weather and at night.

The typical warning signs and pavement markings are just that: typical'. But if field conditions dictate other warning or regulatory items are needed, they may be considered by the Boone County Highway Department or its authorized representative.



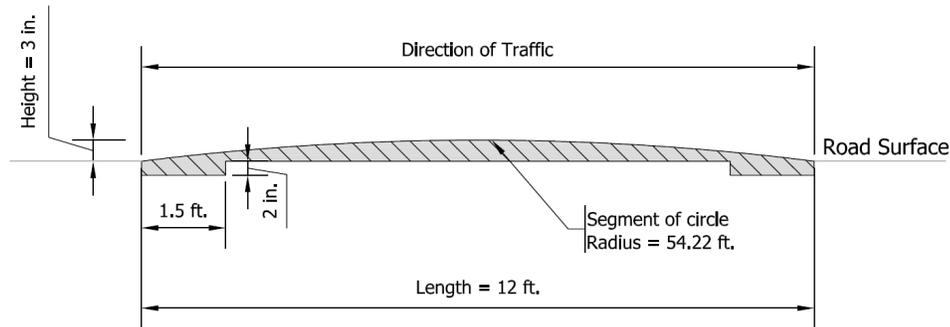
Typical Pavement Markings on Speed Hump

Construction of Speed Humps

The preferred construction material for the installation of speed humps is hot mix asphalt (HMA) surface. Other materials may be considered but only utilized if the street is not already an existing asphalt street. Asphalt, although it is a flexible material, is a preferred material on asphalt streets to

eliminate shoving at the hump in the direction of travel.

Care should be taken during construction/installation of the speed humps. If the humps are installed incorrectly, they will not be effective for speed control. A template will be constructed to verify the accuracy of the hump profile and to ensure that the dimensions of the hump are within reasonable tolerances, normally between 0 in. and 0.25 in.



Cross Section of Speed Hump for Construction

Because speed humps will be installed on existing roads, not on newly constructed streets, the roadway surface will need to be excavated, or milled, at the hump edges to prevent spalling and to gain good adherence to the street materials as shown in the figure above. The speed hump is to be installed in up to two lifts.

Based on current material unit costs and necessary construction to achieve speed hump installation, the estimated cost per speed hump \$5,000 - \$10,000, but pricing could vary due to the number being installed and the location because mobilization and material delivery costs can vary. It is anticipated that a number of speed humps can be installed per day based on clement weather, close proximity of installation, availability of materials, and schedule of contractor.

As this policy has detailed, the Boone County Highway Department is to investigate each request for speed humps to determine if they are warranted. Upon verification of need, sound engineering judgment will be used to determine the number of speed humps and their most effective locations. This care in the design is to avoid the need to remove the speed humps in the future.

Removal of a Speed Humps

Because the speed humps are to be located within Boone County public right-of-way, they may be removed at any time at the direction of the Boone County Board of Commissioners at a cost to Boone County. Speed humps may be removed if deemed by the County to be a public hazard and not being effective at reducing vehicular speeds. The decision to remove will be at the discretion of Boone County based on what is in the best interest of the motoring public.

As it is shown in the *Agreement to Install Speed Humps* in Appendix D, if there is a written fifty-one percent (51%) remonstrance regarding the speed humps in a subdivision after their petitioned installation, they will be removed at a cost to the subdivision homeowner's association.

If the speed humps are to be removed, they can likely be removed at a relatively low cost, in a short

amount of time, with fairly minimal effects on the quality of the roadway. The speed humps are to be milled off with a regular milling machine, with the millings hauled away and disposed of offsite. This will likely cause a rough twelve-foot section of pavement. This rough surface will cause a rumbling until such time that the asphalt heals itself.

Appendix A

Appendix B

Speed Hump Installation Investigation Form

Subdivision:

Road: _____

From: _____

Length: _____

To: _____

Street Width: _____

Curbs: Yes / No

Speed Limit: _____ MPH

A.D.T.: _____ (Minimum: 500, Maximum: 3,000)

Speed Limit Posted: Yes / No

85th% Speed: _____

(Attach Speed Study Reports)

Crash History:	<u>Date</u>	<u>Location</u>	<u>Description</u>
	_____	_____	_____
	_____	_____	_____
	_____	_____	_____
	_____	_____	_____
	_____	_____	_____

Computations for Point System:

ADT	_____
Crash	_____
Speed	_____
Pedestrian	_____
Schools	_____

Minimum Points Needed to Warrant: 25

Total: _____

Speed Humps Warranted: Yes / No

Detailed Sketch: (Include Lot Lines, Existing Street Signs, Storm Sewer Inlet Locations, etc.)

Investigated by: _____

Date: _____

Appendix C

January XX, 2026

Boone County Highway Department
1955 Indianapolis Avenue
Lebanon, IN 46052

RE: Speed Hump Installation

_____ Subdivision
N. of _____ Street/E. of _____
Road _____ Township

To Whom it May Concern:

Our department is in receipt of your submittal regarding the proposed speed hump installation located in _____ Subdivision. Our department has reviewed the locations of the humps, and we do not see any adverse effects pursuant to their installation.

While we understand that the humps will benefit the residents of _____ Subdivision, we also understand that there will be a potential increase in response time to fire emergencies.

We do not object to the installation.

Sincerely,

Fire Department Chief

January XX, 2026

Boone County Highway Department
1955 Indianapolis Avenue
Lebanon, IN 46052

RE: Speed Hump Installation

_____ Subdivision
N. of _____ Street/E. of _____
Road _____ Township

To Whom it May Concern:

Our department is in receipt of your submittal regarding the proposed speed hump installation located in _____ Subdivision. Our department has reviewed the locations of the humps, and we do not see any adverse effects pursuant to their installation.

While we understand that the humps will benefit the residents of _____ Subdivision, we also understand that there will be a potential increase in response time to police emergencies.

We do not object to the installation.

Sincerely,

Boone County Sheriff

January XX, 2026

Boone County Highway Department
1955 Indianapolis Avenue
Lebanon, IN 46052

RE: Speed Hump Installation

_____ Subdivision
N. of _____ Street/E. of _____
Road _____ Township

To Whom it May Concern:

Our department is in receipt of your submittal regarding the proposed speed hump installation located in _____ Subdivision. Our department has reviewed the locations of the humps, and we do not see any adverse effects pursuant to their installation.

While we understand that the humps will benefit the residents of _____ Subdivision, we also understand that there could be a potential increase in travel time for school buses.

We do not object to the installation.

Sincerely,

School Superintendent

_____ School District

Appendix D

AGREEMENT TO INSTALL SPEED HUMPS

This Agreement entered into on the dates set out herein by _____
_____ (the "Association"), and the Board of Commissioners of Boone County,
(the "County").

WITNESS THAT:

WHEREAS, the parties are desirous of installing speed humps in _____ Subdivision in
_____ Township, Boone County, Indiana (hereinafter referred to as the "Project"); and,

WHEREAS, the Association is desirous of entering into an agreement with the County, whereby the County will provide certain labor, equipment, and material necessary to install said speed humps and the Association will pay for the costs of the project to install said speed humps.

IT IS THEREBY AGREED by the parties as follows:

1. The Project includes installing _____ asphalt speed humps to be placed at locations as directed by the Boone County Highway Department or their authorized representative.
2. The County shall provide all construction project management services, all asphalt and other materials, all labor and equipment, all construction inspection, and all contract administration for the Project.
3. The Association will pay to the County, _____ Dollars (\$ _____) to cover the costs of the engineering and traffic studies, construction materials, labor, and equipment for the installation of the speed humps, and any necessary street signs and pavement markings required.
4. The parties agree that \$ _____ is an estimate of the costs of the materials and labor. However, in the event said costs are less than estimated, the County shall apply the balance of the \$ _____ to reimburse the other costs for the Project such as engineering and traffic studies.
5. It is agreed that payment in the amount of \$ _____, will be made by the Association to the Board of Commissioners of Boone County thirty (30) days from the execution of this agreement.
6. It is agreed that the County, subject to contractor availability, will complete the installation during the summer construction season of _____, but may, at its option, delay work until calendar year _____ to coordinate this project with others similar installations in the area to obtain the best project results and to obtain the bid prices used in the estimate for this agreement.
7. The parties agree that the County may terminate this agreement and remove the speed humps from said Project, at the County's expense, if deemed necessary.
8. The parties agree that, if in the future, there is a written remonstrance from fifty-one percent (51%) of the homeowner's regarding the speed humps remaining in the neighborhood, the Association or its individual members can petition the County to have the humps removed, at the expense of the Association.
9. The County agrees to maintain the street signs and the pavement markings at the expense of the County; until such time that the hump is removed.

IN WITNESS WHEREOF, the Association has set their hands and seals this _____ day of _____
_____ and the Board of Commissioners of Boone County has
approved this Agreement in open meeting on the _____ day of _____.

Homeowners' Association: _____

President: _____

Treasurer: _____

Date: _____

Before me, a Notary Public and for said County and State, personally appeared _____
_____ who acknowledged the execution of the foregoing Agreement to be
their voluntary act and deed, and who, having been duly sworn, stated that any representation therein contained
are true.

Witness my hand and Notarial Seal this _____ day of _____

My commission expires:

Signature: _____

Printed: _____ Notary Public Resident of _
County, Indiana

Tim Beyer, District 1

Don Lawson, District 2

Scott Pell, District 3

Attest: _____
Debbie Morton-Crum, Auditor

Date: _____

Appendix E



Boone County Speed Humps – What Residents Should Know

Speed humps are traffic-calming features used on residential streets to reduce vehicle speeds and improve neighborhood safety. This brochure provides key points on how Boone County evaluates, installs, and manages speed humps.

Why Speed Humps Are Used

- Reduce excessive speeding
- Discourage cut-through traffic in neighborhoods
- Improve safety for pedestrians and cyclists

Where Speed Humps May Be Installed

Speed humps are considered only on local residential streets with documented speeding or safety concerns. They are not installed on arterial or collector roads.

What Speed Humps Do – and Do Not Do

Speed humps are effective at reducing vehicle speeds but may increase travel time slightly and can affect emergency response and service vehicles. These impacts should be carefully evaluated before installation.

How to Request Speed Humps

Residents must submit a petition signed by at least 75% of property owners in the affected area. Boone County Highway then performs a traffic and safety study to determine eligibility.

Costs and Maintenance

Installation costs are paid by the requesting homeowners association or residents. Once installed, the Boone County Highway Department maintains approved speed humps as part of routine roadway maintenance.

Removal

Speed humps may be removed if safety concerns arise or if a majority of residents request removal.

Questions?

For more information, contact:
Boone County Highway Department
Lebanon, Indiana
(765) 482-4550
highway@co.boone.in.us
www.boonecounty.in.gov/offices/highway