

**BOONE COUNTY HIGHWAY DEPARTMENT**  
**STANDARD DRAWINGS**



**Boone County**

THESE BOONE COUNTY HIGHWAY DEPARTMENT STANDARD DRAWINGS ARE  
APPROVED ON THIS 16<sup>th</sup> DAY OF JUNE, 2025 BY:

THE BOONE COUNTY HIGHWAY DEPARTMENT:

BY: Nicholas D. Parr  
Nick Parr, Director of Highways

6/16/2025  
Date

THE BOONE COUNTY BOARD OF COMMISSIONERS:

BY: [Signature]  
Scott Pell, President

06/16/2025  
Date

[Signature]  
Donnie Lawson, Member

06/16/2025  
Date

[Signature]  
Tim Beyer, Member

06/16/2025  
Date

ATTEST: [Signature]  
Debbie Crum, County Auditor

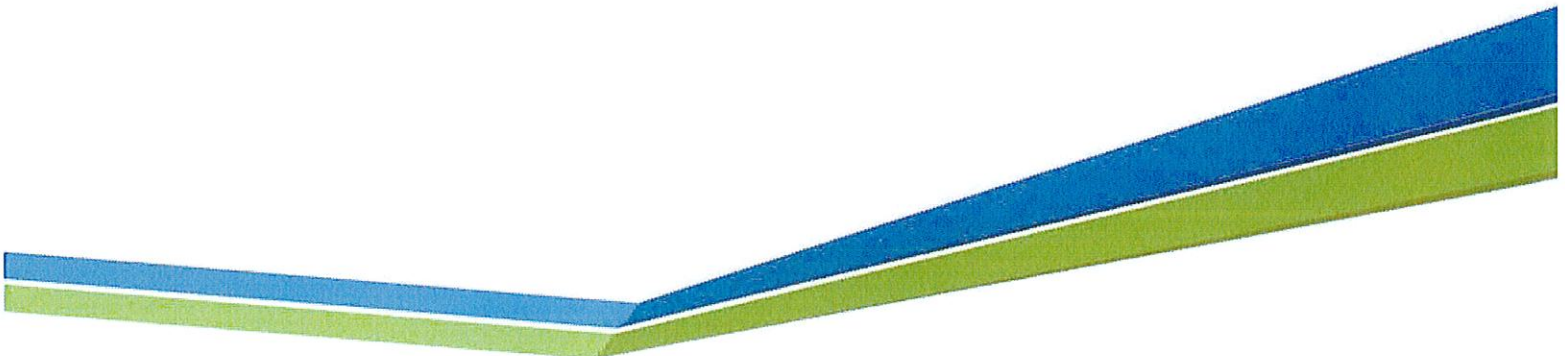
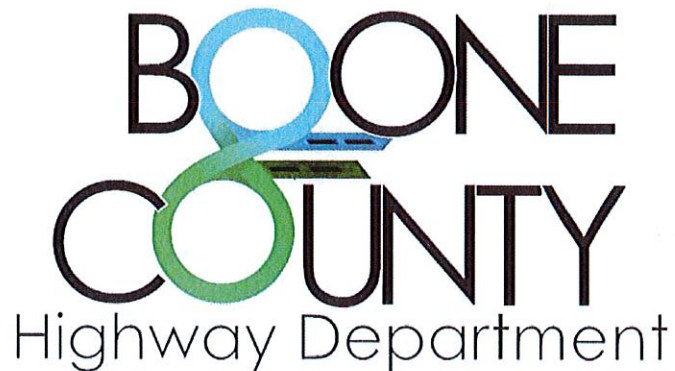
06/16/2025  
Date

ALL PREVIOUS STANDARD DRAWINGS ARE HEREBY REPEALED AND REPLACED  
WITH THESE STANDARD DRAWINGS, EFFECTIVE ON THIS 16<sup>th</sup> DAY OF JUNE, 2025

BOONE COUNTY HIGHWAY DEPARTMENT  
1955 INDIANAPOLIS AVENUE  
LEBANON, IN 46052

## INDEX OF BOONE COUNTY STANDARD DRAWINGS

Section 100	General Provisions (Not Used)
Section 200	Earthwork (Not Used)
Section 300	Aggregate Pavements And Bases
Section 400	Asphalt Pavements
Section 500	Concrete Pavements (Not Used)
Section 600	Incidental Construction
Section 700	Structures
Section 800	Traffic Control Devices
Section 900	Street Signs



INDEX OF BOONE COUNTY STANDARDS  
200 SERIES STANDARD DRAWINGS  
EARTHWORK

NOT USED



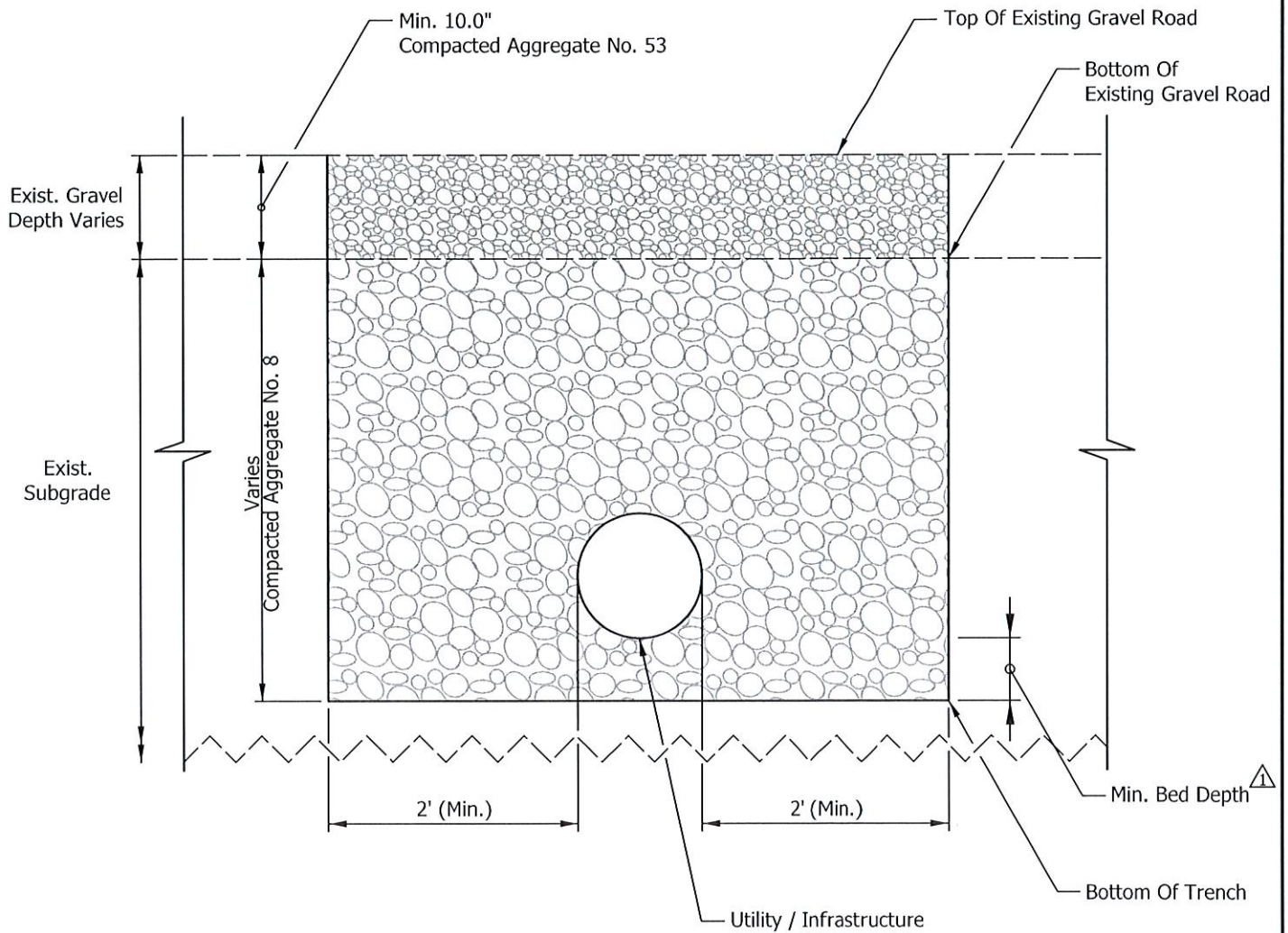


INDEX OF BOONE COUNTY STANDARDS  
300 SERIES STANDARD DRAWINGS  
AGGREGATE PAVEMENTS AND BASES

Typical Gravel Road Cut Patch Detail.....BC-302-001



3/27/2019 7:23:52 AM I:\1720004 - Boone County As-Needed Assistance\Standards Consolidation\09Road\04 Design Files\02 DGN\Plans\300a\302-001\_BC\_Full Depth Gravel Road Cut\_Typical.dgn



NOTES:



Bed Depth	
Pipe Diameter (in.)	Min. Bed Depth (in.)
6" To 15"	6"
18" And Above	8"

BOONE COUNTY HIGHWAY DEPARTMENT



TYPICAL GRAVEL ROAD CUT PATCH DETAIL

STANDARD DRAWING

NO. BC-302-001

SHEET 1 OF 1

SCALE: NO SCALE

**INDEX OF BOONE COUNTY STANDARDS  
400 SERIES STANDARD DRAWINGS  
ASPHALT PAVEMENTS**

Typical Rural Arterial Street Cross Section.....	BC-400-001
Typical Urban Arterial Street With Raised Median Cross Section.....	BC-400-002A
Typical Urban Arterial Street With Striped Median Cross Section.....	BC-400-002B
Typical Rural Major Collector Cross Section.....	BC-400-003
Typical Urban Major Collector With Raised Median Cross Section.....	BC-400-004A
Typical Urban Major Collector With Striped Median Cross Section.....	BC-400-004B
Typical Rural Minor Collector Street Cross Section.....	BC-400-005
Typical Urban Minor Collector Street Cross Section.....	BC-400-006
Typical Rural Local Street/Residential Feeder Cross Section.....	BC-400-007
Typical Urban Local Street/Residential Feeder Cross Section.....	BC-400-008
Typical Underdrain Detail.....	BC-401-001
Typical Full Depth Patch Detail.....	BC-402-001
Typical HMA Road Cut Patch Detail.....	BC-402-002
Terminus Transitions.....	BC-403-001
Typical Safety Edge Details.....	BC-403-002
Aggregate to HMA Pavement Transition Detail.....	BC-403-003



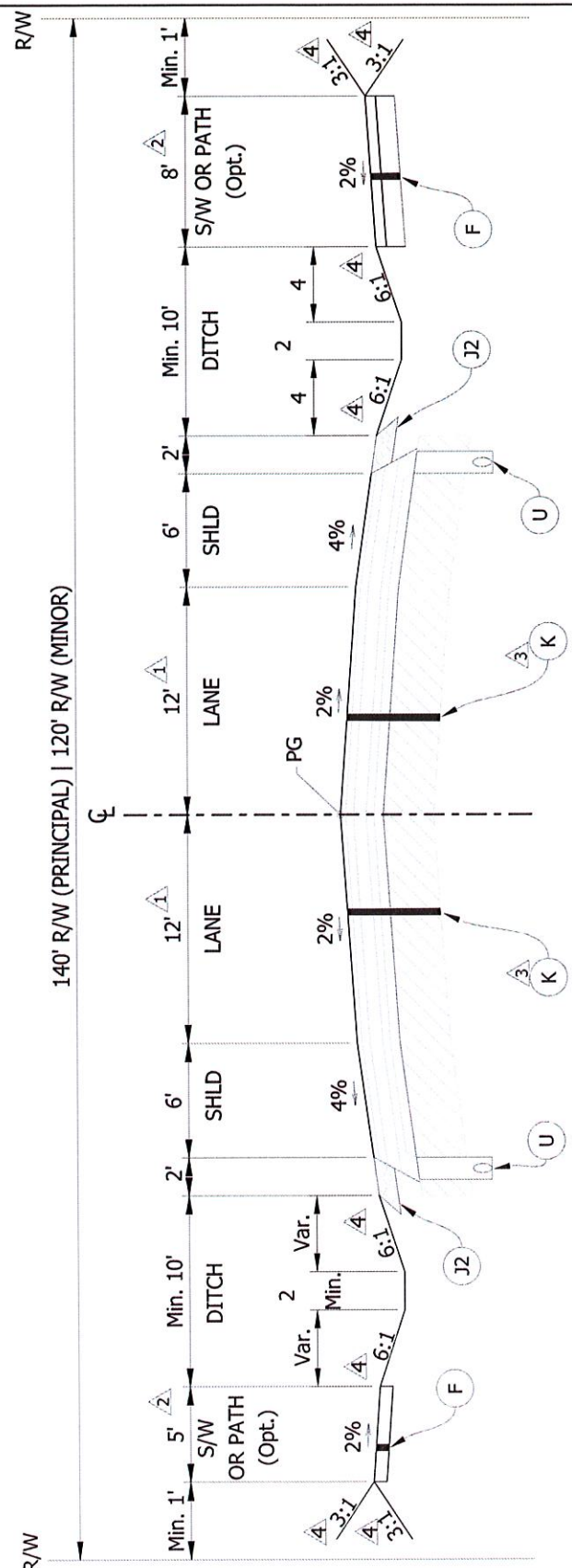




BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL RURAL ARTERIAL STREET  
CROSS SECTION

STANDARD DRAWING  
NO. BC-400-001  
SHEET 1 OF 1  
SCALE: NO SCALE



NOTES:

- See Boone County Thoroughfare Plan;  
2 Lane Roadway - 1 x 12' Lane Each Direction  
4 Lane Roadway - 2 x 12' Lanes Each Direction  
Addition Of Auxilliary Lane - 12'  
Addition Of Optional Bike Lane - 5'
- See Boone County Thoroughfare Plan;  
Both Sides Optional 5' Sidewalk Or 8' - 12' Multi-use Path
- Tack Coat To Be Applied Between Pavement Lifts And On All Vertical Surfaces
- Foreslopes And Backslopes Shall Be In Accordance With Clear Zone Requirements Set Forth By INDOT Design Manual (Current Edition)
- For Safety Edge Details, See STD Drawing BC-403-002

LEGEND:

- F Concrete Sidewalk - See STD Drawing BC-602-001 Or  
HMA Path - See STD Drawing BC-602-002
- J2 6.0" Compacted Aggregate Shoulder
- K Underdrain (Typ.) - See STD Drawing BC-401-001
- U

Pavement Layer	Bridge Rehabilitation/ Replacement Approach	New Construction or Expansion
HMA Surface, 9.5mm, Type B	1.5"	1.5"
HMA Intermediate, 12.5mm, Type B	2.0"	2.5"
HMA Base 19.0mm, Type B	3.0"	4.0"
HMA Intermediate OG, 19.0mm, Type B	-	2.5"
Compacted Aggregate No. 53	6.0"	3.0"
INDOT Subgrade Treatment IB or IC	Yes	Yes

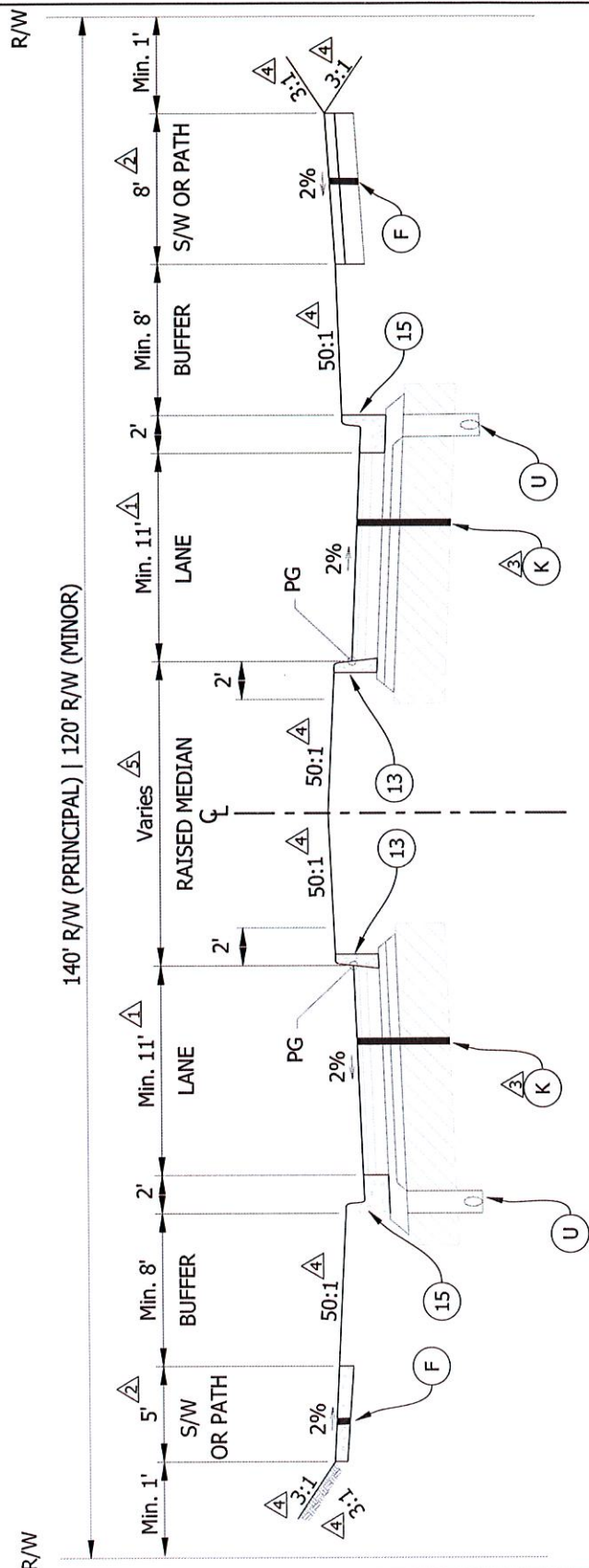
RECOMMENDED  
FOR APPROVAL

DESIGN ENGINEER

REVISED 07/05/2022







**NOTES:**

- ① See Boone County Thoroughfare Plan;  
2 Lane Roadway - 1 x 11' Lane Each Direction (Min.)  
4 Lane Roadway - 2 x 11' Lanes Each Direction (Min.)  
Addition Of Auxiliary Lane - 11' (Min.)  
Addition Of Optional Bike Lane - 5'
- ② See Boone County Thoroughfare Plan;  
Both Sides Require 5' Sidewalk Or 8' - 12' Multi-use Path
- ③ Tack Coat To Be Applied Between Pavement Lifts And On All Vertical Surfaces
- ④ Foreslopes And Backslopes Shall Be In Accordance With Clear Zone Requirements Set Forth By INDOT Design Manual (Current Edition)
- ⑤ See Boone County Thoroughfare Plan;  
Optional Raised Median 3' - 16'  
When Median < 3' Use STD Drawing BC-400-002B

**LEGEND:**

- ① Concrete Sidewalk - See STD Drawing BC-602-001 Or HMA Path - See STD Drawing BC-602-002
- ② Concrete Sidewalk - See STD Drawing BC-602-002
- ③ Concrete Sidewalk - See STD Drawing BC-602-002
- ④ Concrete Sidewalk - See STD Drawing BC-602-002
- ⑤ Concrete Sidewalk - See STD Drawing BC-602-002

Pavement Layer	Bridge Rehabilitation/ Replacement Approach	New Construction or Expansion
HMA Surface, 9.5mm, Type B	1.5"	1.5"
HMA Intermediate, 12.5mm, Type B	2.0"	2.5"
HMA Base 19.0mm, Type B	3.0"	4.0"
HMA Intermediate OG, 19.0mm, Type B	-	2.5"
Compacted Aggregate No. 53	6.0"	3.0"
INDOT Subgrade Treatment 1B or 1C	Yes	Yes

Underdrain (Typ.) - See STD Drawing BC-401-001

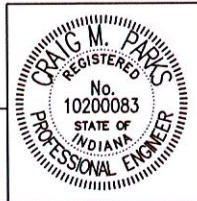
Concrete Curb - See STD Drawing BC-601-001

Concrete Curb And Gutter - See STD Drawing BC-601-001

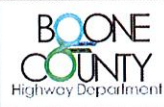
RECOMMENDED FOR APPROVAL

*[Signature]*  
DESIGN ENGINEER

REVISED 07/05/2022

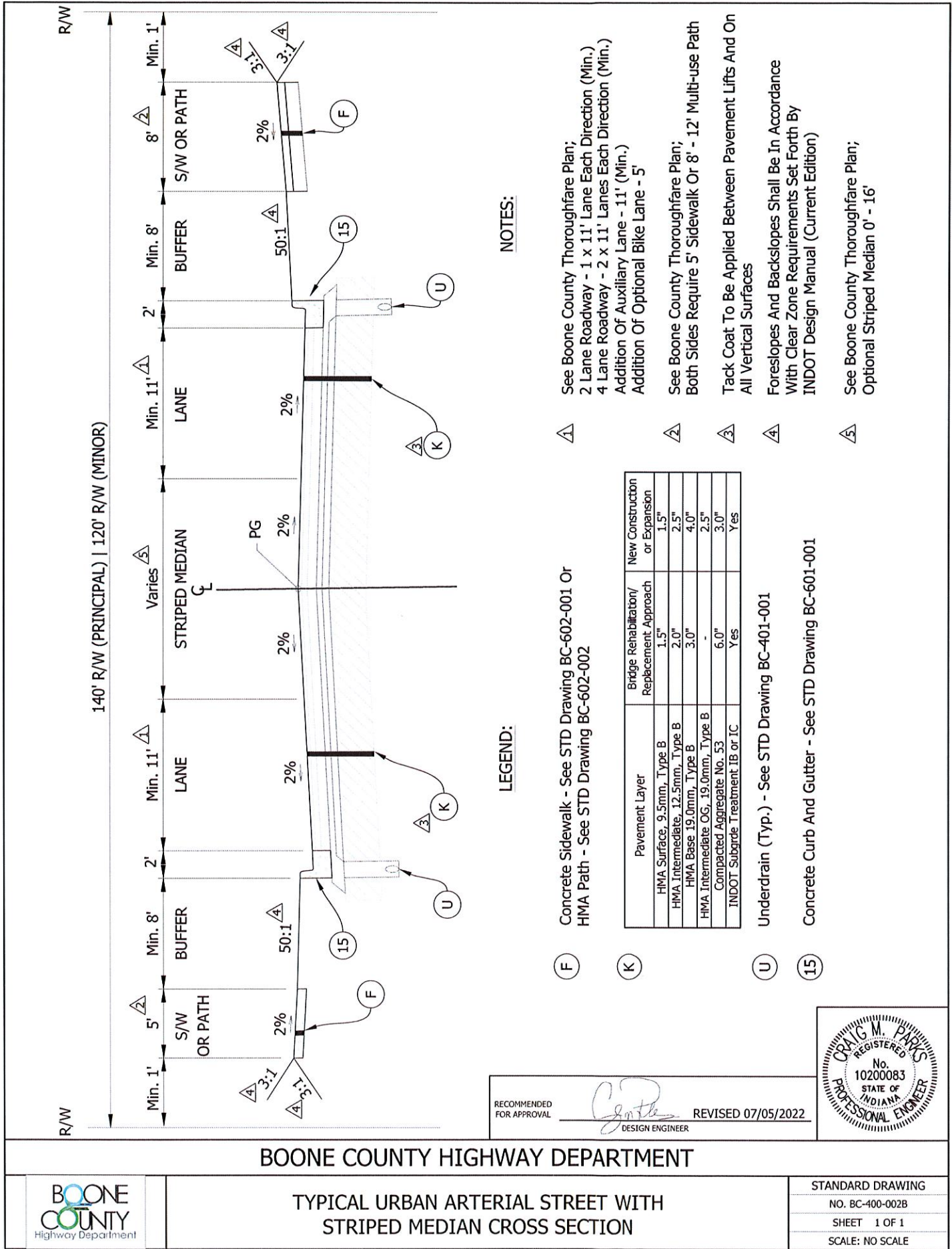


**BOONE COUNTY HIGHWAY DEPARTMENT**



**TYPICAL URBAN ARTERIAL STREET WITH  
RAISED MEDIAN CROSS SECTION**

STANDARD DRAWING  
NO. BC-400-002A  
SHEET 1 OF 1  
SCALE: NO SCALE

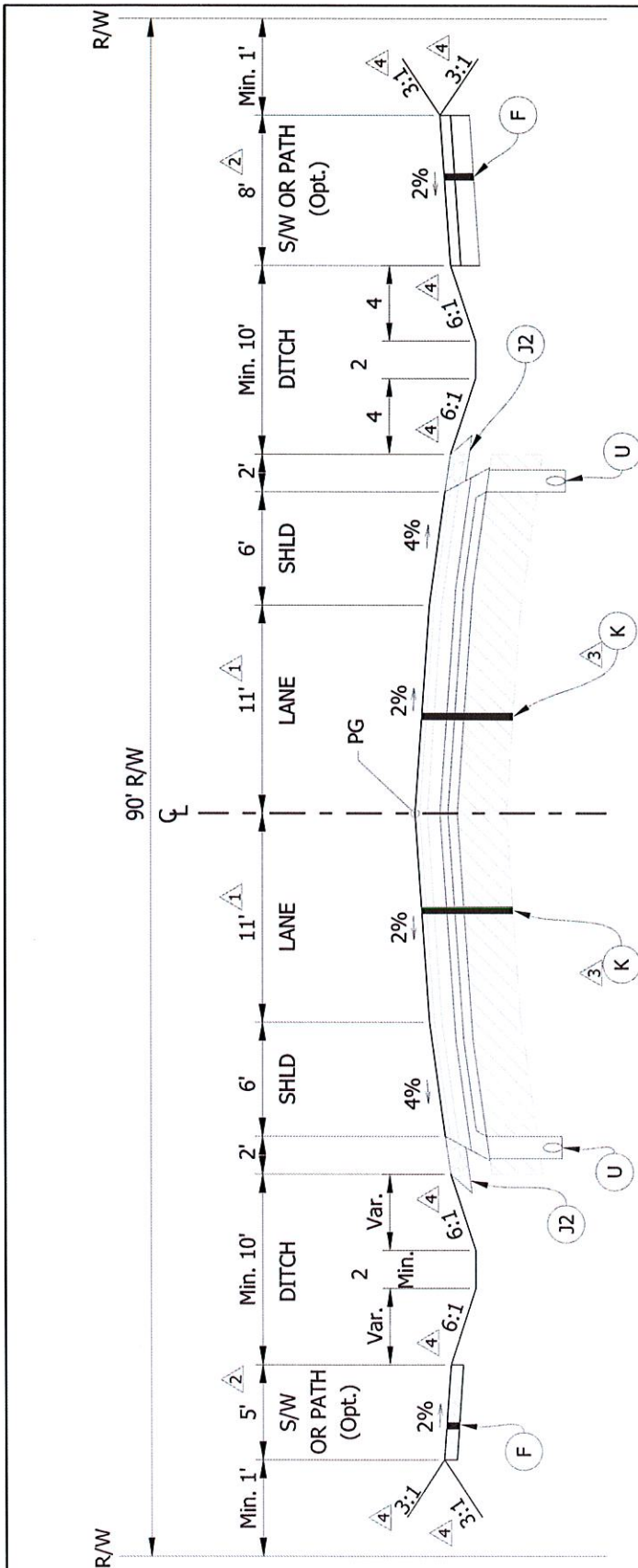


BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL URBAN ARTERIAL STREET WITH STRIPED MEDIAN CROSS SECTION

STANDARD DRAWING  
NO. BC-400-002B  
SHEET 1 OF 1  
SCALE: NO SCALE





# NOTES:

# LEGEND:

- See Boone County Thoroughfare Plan;  
2 Lane Roadway - 1 x 11' Lane Each Direction  
Addition Of Optional Bike Lane - 5'  
Addition Of Auxiliary Lane - 11'
- See Boone County Thoroughfare Plan;  
Both Sides Optional 5' Sidewalk Or 8' - 12' Multi-use Path
- Tack Coat To Be Applied Between Pavement Lifts And On  
All Vertical Surfaces
- Foreslopes And Backslopes Shall Be In Accordance  
With Clear Zone Requirements Set Forth By  
INDOT Design Manual (Current Edition)
- For Safety Edge Details, See STD Drawing BC-403-002

Concrete Sidewalk - See STD Drawing BC-602-001 Or  
HMA Path - See STD Drawing BC-602-002

6.0" Compacted Aggregate Shoulder

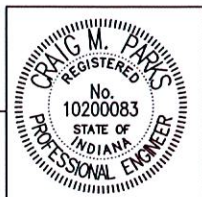
Pavement Layer	Bridge Rehabilitation/ Replacement Approach	New Construction or Expansion
HMA Surface, 9.5mm, Type B	1.5"	1.5"
HMA Intermediate, 12.5mm, Type B	2.0"	2.5"
HMA Base 19.0mm, Type B	3.0"	4.0"
HMA Intermediate OG, 19.0mm, Type B	-	2.5"
Compacted Aggregate No. 53	6.0"	3.0"
INDOT Subgrade Treatment TB or IC	Yes	Yes

Underdrain (Typ.) - See STD Drawing BC-401-001

RECOMMENDED  
FOR APPROVAL

*[Signature]*  
DESIGN ENGINEER

REVISED 07/05/2022

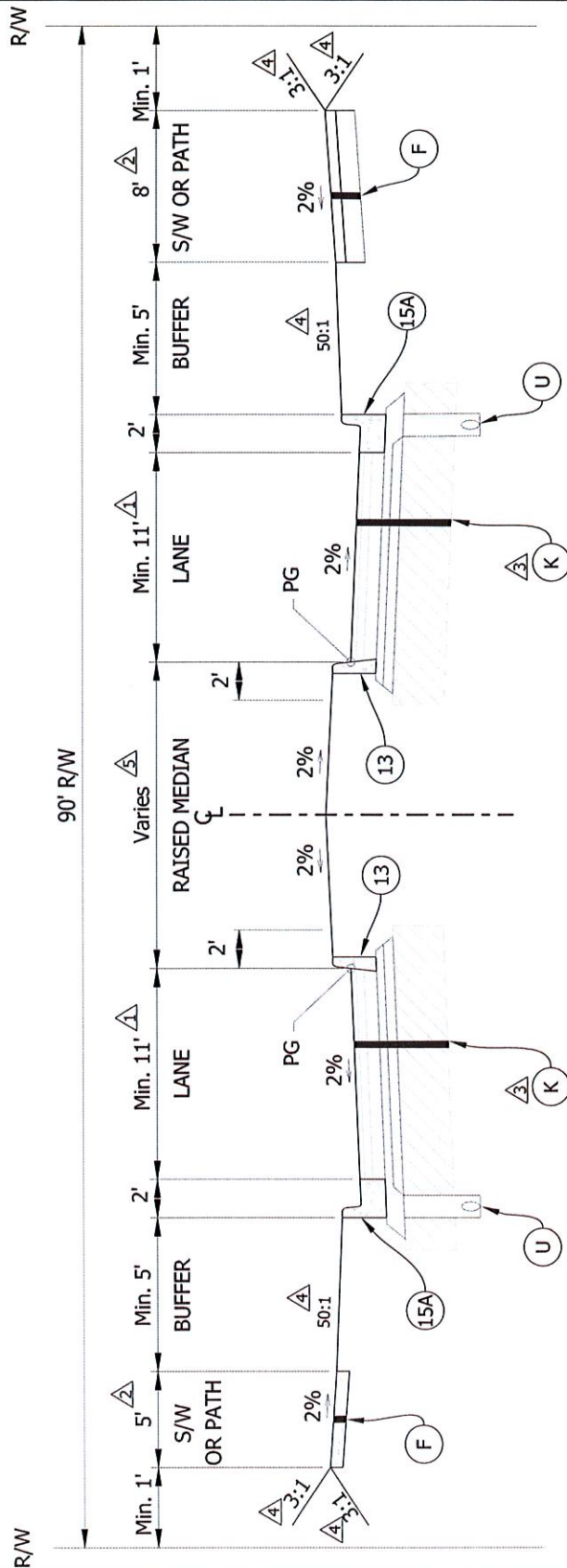


## BOONE COUNTY HIGHWAY DEPARTMENT

## TYPICAL RURAL MAJOR COLLECTOR STREET CROSS SECTION



STANDARD DRAWING  
NO. BC-400-003  
SHEET 1 OF 1  
SCALE: NO SCALE



# NOTES:

- 1 See Boone County Thoroughfare Plan;  
2 Lane Roadway - 1 x 11' Lane Each Direction (Min.)  
Addition Of Optional Bike Lane - 5'  
Addition Of Auxiliary Lane - 11' (Min.)  
Addition Of Optional Parking Lane - 8' (Min.)
- 2 See Boone County Thoroughfare Plan;  
Both Sides Require 5' Sidewalk Or 8' - 12' Multi-use Path
- 3 Tack Coat To Be Applied Between Pavement Lifts And On All Vertical Surfaces
- 4 Foreslopes And Backslopes Shall Be In Accordance With Clear Zone Requirements Set Forth By INDOT Design Manual (Current Edition)
- 5 See Boone County Thoroughfare Plan;  
Optional Raised Median 3' - 16'  
When Median < 3' Use STD Drawing BC-400-004B

# LEGEND:

- Concrete Sidewalk - See STD Drawing BC-602-001 Or
- HMA Path - See STD Drawing BC-602-002

Pavement Layer	Bridge Rehabilitation/ Replacement Approach	New Construction or Expansion
HMA Surface, 9.5mm, Type B	1.5"	1.5"
HMA Intermediate, 12.5mm, Type B	2.0"	2.5"
HMA Base 19.0mm, Type B	3.0"	4.0"
HMA Intermediate OG, 19.0mm, Type B	-	2.5"
Compacted Aggregate No. 53	6.0"	3.0"
INDOT Subgrade Treatment IB or IC	Yes	Yes

- Underdrain (Typ.) - See STD Drawing BC-401-001
- Mountable Concrete Curb And Gutter (Typ.) - See STD Drawing BC-601-001
- Concrete Curb - See STD Drawing BC-601-001

RECOMMENDED  
FOR APPROVAL

*Design Engineer*  
DESIGN ENGINEER

REVISED 07/05/2022



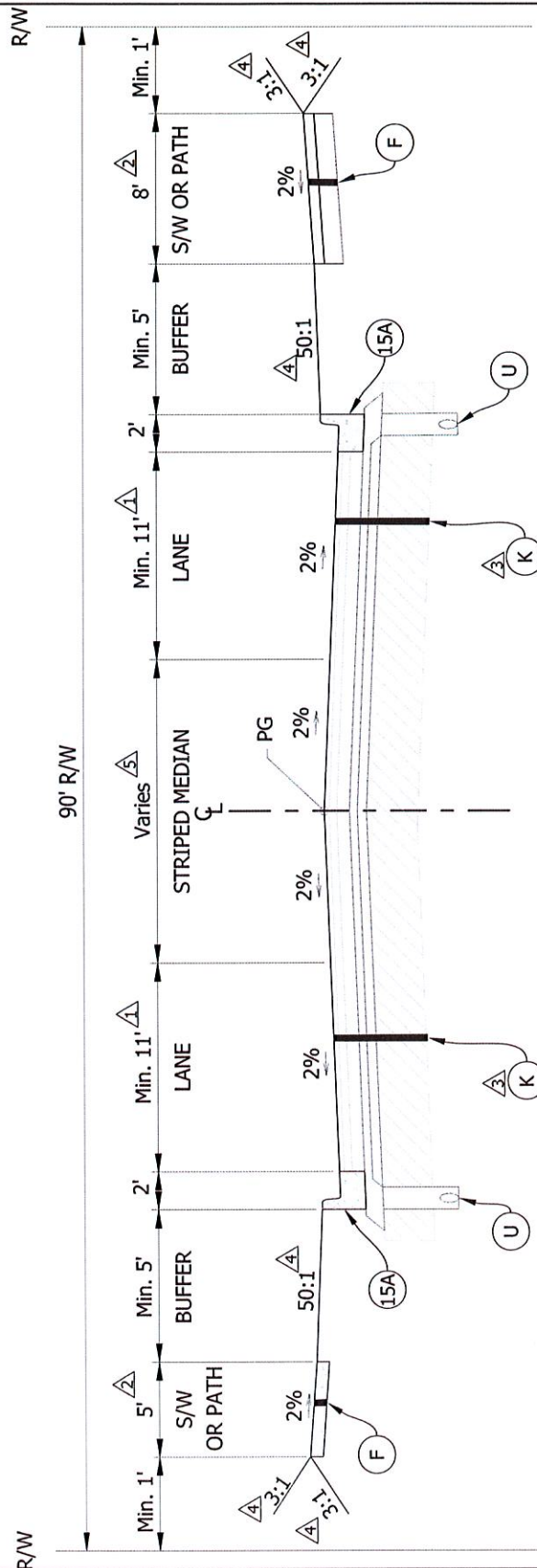
## BOONE COUNTY HIGHWAY DEPARTMENT

## TYPICAL URBAN MAJOR COLLECTOR STREET WITH RAISED MEDIAN CROSS SECTION



STANDARD DRAWING  
NO. BC-400-004A  
SHEET 1 OF 1  
SCALE: NO SCALE





# LEGEND:

- (F) Concrete Sidewalk - See STD Drawing BC-602-001 Or HMA Path - See STD Drawing BC-602-002
- (K) Pavement Layer
- (U) Underdrain (Typ.) - See STD Drawing BC-401-001
- (15A) Mountable Concrete Curb And Gutter (Typ.) - See STD Drawing BC-601-001

# NOTES:

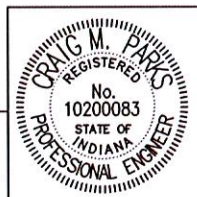
- 1 See Boone County Thoroughfare Plan; 2 Lane Roadway - 1 x 11' Lane Each Direction (Min.) Addition Of Optional Bike Lane - 5' Addition Of Optional Parking Lane - 8' (Min.)
- 2 See Boone County Thoroughfare Plan; Both Side Require 5' Sidewalk Or 8' - 12' Multi-use Path
- 3 Tack Coat To Be Applied Between Pavement Lifts And On All Vertical Surfaces
- 4 Foreslopes And Backslopes Shall Be In Accordance With Clear Zone Requirements Set Forth By INDOT Design Manual (Current Edition)
- 5 See Boone County Thoroughfare Plan; Optional Striped Median 0' - 16'

Pavement Layer	Bridge Rehabilitation/ Replacement Approach	New Construction or Expansion
HMA Surface, 9.5mm, Type B	1.5"	1.5"
HMA Intermediate, 12.5mm, Type B	2.0"	2.5"
HMA Base 19.0mm, Type B	3.0"	4.0"
HMA Intermediate OG, 19.0mm, Type B	-	2.5"
Compacted Aggregate No. 53	6.0"	3.0"
INDOT Subgrade Treatment IB or IC	Yes	Yes

RECOMMENDED FOR APPROVAL

*[Signature]*  
DESIGN ENGINEER

REVISED 07/05/2022

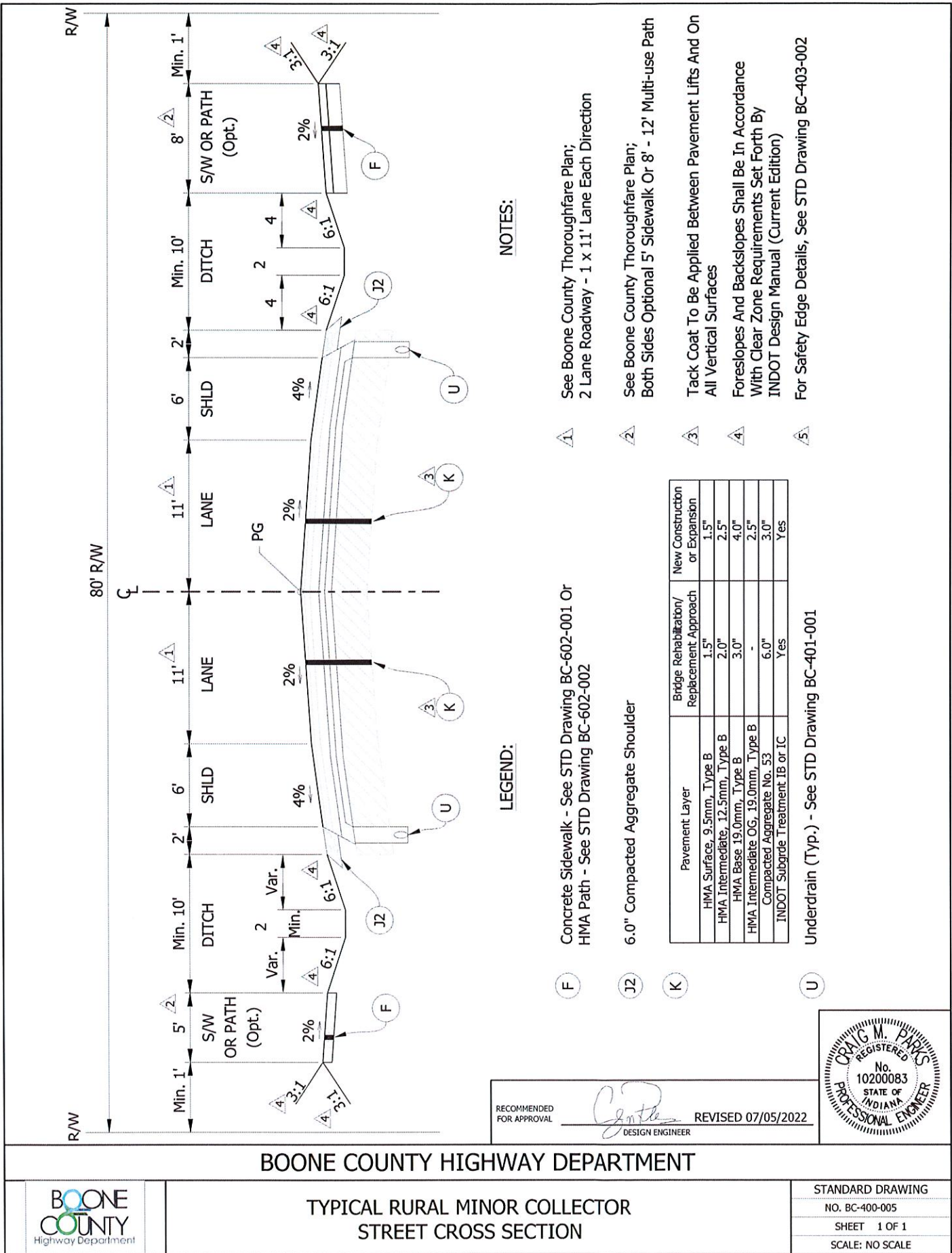


## BOONE COUNTY HIGHWAY DEPARTMENT

## TYPICAL URBAN MAJOR COLLECTOR STREET WITH STRIPED MEDIAN CROSS SECTION



STANDARD DRAWING  
NO. BC-400-004B  
SHEET 1 OF 1  
SCALE: NO SCALE



NOTES:

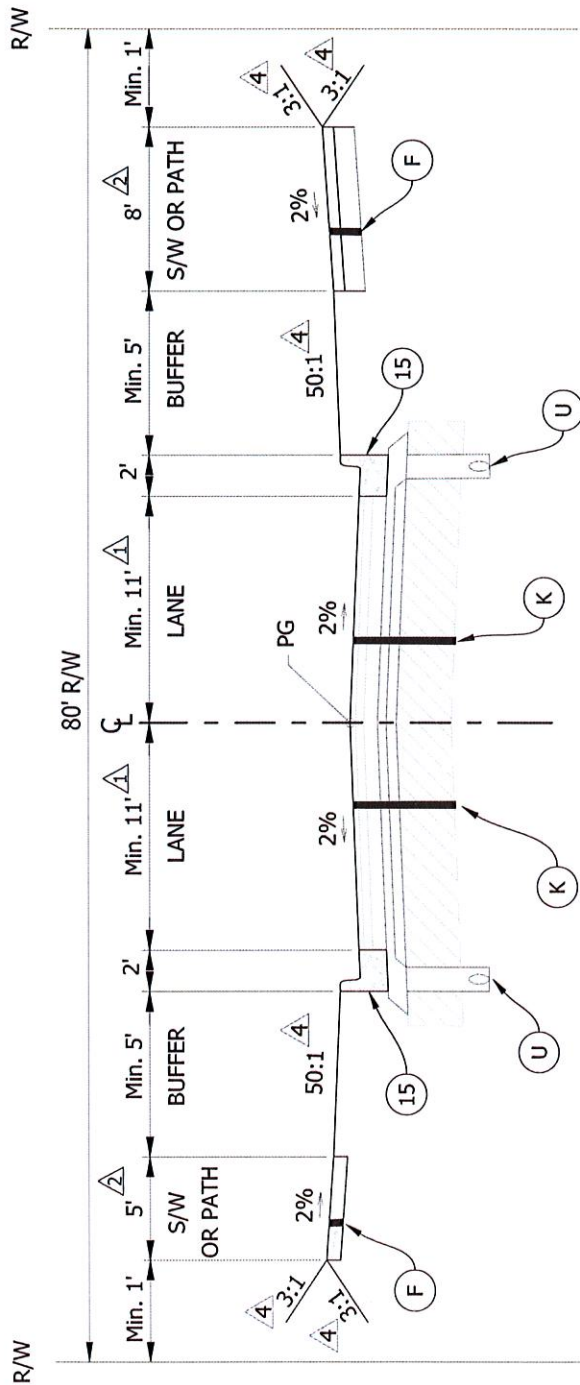
- 1 See Boone County Thoroughfare Plan;  
2 Lane Roadway - 1 x 11' Lane Each Direction
- 2 See Boone County Thoroughfare Plan;  
Both Sides Optional 5' Sidewalk Or 8' - 12' Multi-use Path
- 3 Tack Coat To Be Applied Between Pavement Lifts And On All Vertical Surfaces
- 4 Foreslopes And Backslopes Shall Be In Accordance With Clear Zone Requirements Set Forth By INDOT Design Manual (Current Edition)
- 5 For Safety Edge Details, See STD Drawing BC-403-002

LEGEND:

- F Concrete Sidewalk - See STD Drawing BC-602-001 Or HMA Path - See STD Drawing BC-602-002
- J2 6.0" Compacted Aggregate Shoulder
- K Bridge Rehabilitation/Replacement Approach
- U Underdrain (Typ.) - See STD Drawing BC-401-001

Pavement Layer	Bridge Rehabilitation/Replacement Approach	New Construction or Expansion
HMA Surface, 9.5mm, Type B	1.5"	1.5"
HMA Intermediate, 12.5mm, Type B	2.0"	2.5"
HMA Base 19.0mm, Type B	3.0"	4.0"
HMA Intermediate OG, 19.0mm, Type B	-	2.5"
Compacted Aggregate No. 53	6.0"	3.0"
INDOT Subgrade Treatment IB or IC	Yes	Yes





# NOTES:

- See Boone County Thoroughfare Plan;  
2 Lane Roadway - 1 x 11' Lane Each Direction (Min.)  
Addition Of Optional Bike Lane - 4'  
Addition Of Optional Parking Lane - 8' (Min.)
- See Boone County Thoroughfare Plan;  
Both Sides Require 5' Sidewalk Or 8' - 12' Multi-use Path
- Tack Coat To Be Applied Between Pavement Lifts And On All Vertical Surfaces
- Foreslopes And Backslopes Shall Be In Accordance With Clear Zone Requirements Set Forth By INDOT Design Manual (Current Edition)

1

Concrete Sidewalk - See STD Drawing BC-602-001 Or  
HMA Path - See STD Drawing BC-602-002

F

2

Pavement Layer	Bridge Rehabilitation/ Replacement Approach	New Construction or Expansion
HMA Surface, 9.5mm, Type B	1.5"	1.5"
HMA Intermediate, 12.5mm, Type B	2.0"	2.5"
HMA Base 19.0mm, Type B	3.0"	4.0"
HMA Intermediate OG, 19.0mm, Type B	-	2.5"
Compacted Aggregate No. 53	6.0"	3.0"
INDOT Subgrade Treatment IB or IC	Yes	Yes

K

3

Underdrain (Typ.) - See STD Drawing BC-401-001

U

4

Concrete Curb And Gutter - See STD Drawing BC-601-001

15

RECOMMENDED  
FOR APPROVAL

*[Signature]*  
DESIGN ENGINEER

REVISED 07/05/2022

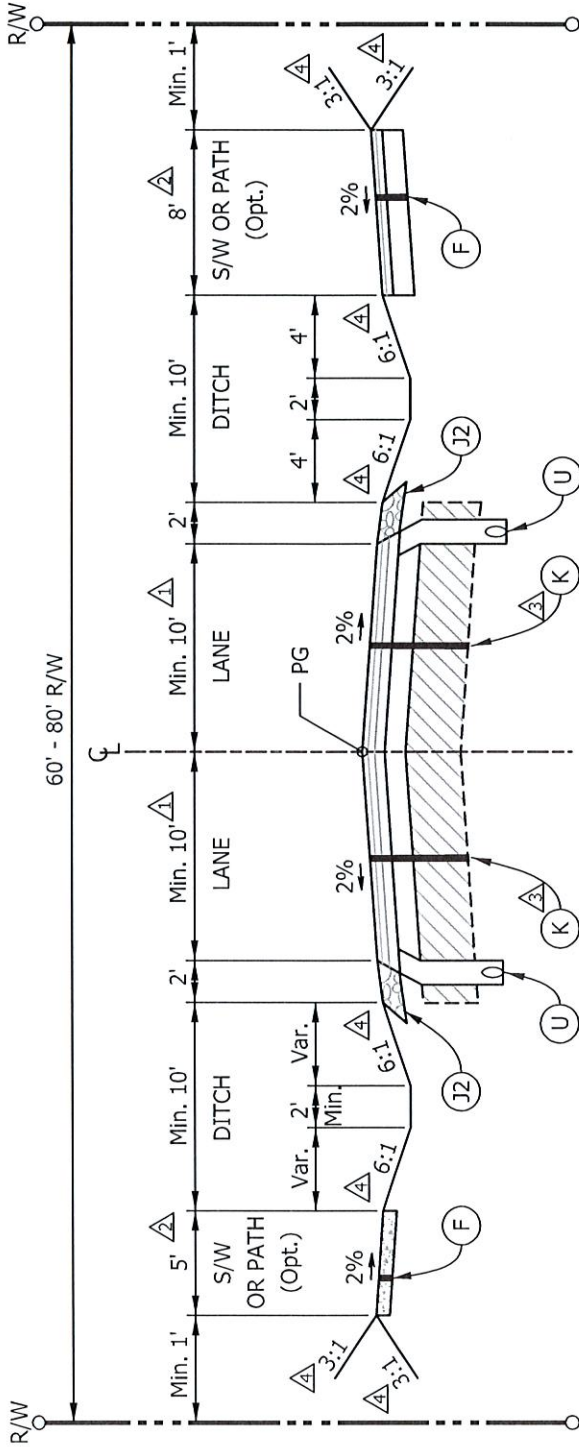


## BOONE COUNTY HIGHWAY DEPARTMENT

### TYPICAL URBAN MINOR COLLECTOR STREET CROSS SECTION



STANDARD DRAWING  
NO. BC-400-006  
SHEET 1 OF 1  
SCALE: NO SCALE



**LEGEND:**

(F) Concrete Sidewalk - See STD Drawing BC-602-001 Or  
HMA Path - See STD Drawing BC-602-002

(J2) 6.0" Compacted Aggregate Shoulder

Pavement Layer	Local Streets	Residential Feeder
HMA Surface, 9.5mm, Type B	1.5"	1.5"
HMA Intermediate, 12.5mm, Type B	2.0"	2.5"
HMA Base, 19.0mm, Type B	3.0"	4.0"
HMA Intermediate OG, 19.0mm, Type B	-	-
Compacted Aggregate No. 53	6.0"	8.0"
INDOT Subgrade Treatment, IB or IC	Yes	Yes

(U) Underdrain (Typ.) - See STD Drawing BC-401-001

**NOTES:**

(A) See Boone County Thoroughfare Plan;  
2 Lane Roadway - 1 x 10' Lane Each Direction (Min.)

(A) See Boone County Thoroughfare Plan;  
Both Sides Optional 5' Sidewalk Or 8' - 12' Multi-use Path

(A) Tack Coat To Be Applied Between Pavement Lifts And On  
All Vertical Surfaces

(A) Foreslopes And Backslopes Shall Be In Accordance  
With Clear Zone Requirements Set Forth By  
INDOT Design Manual (Current Edition)

5. For Safety Edge Details, See STD Drawing BC-403-002

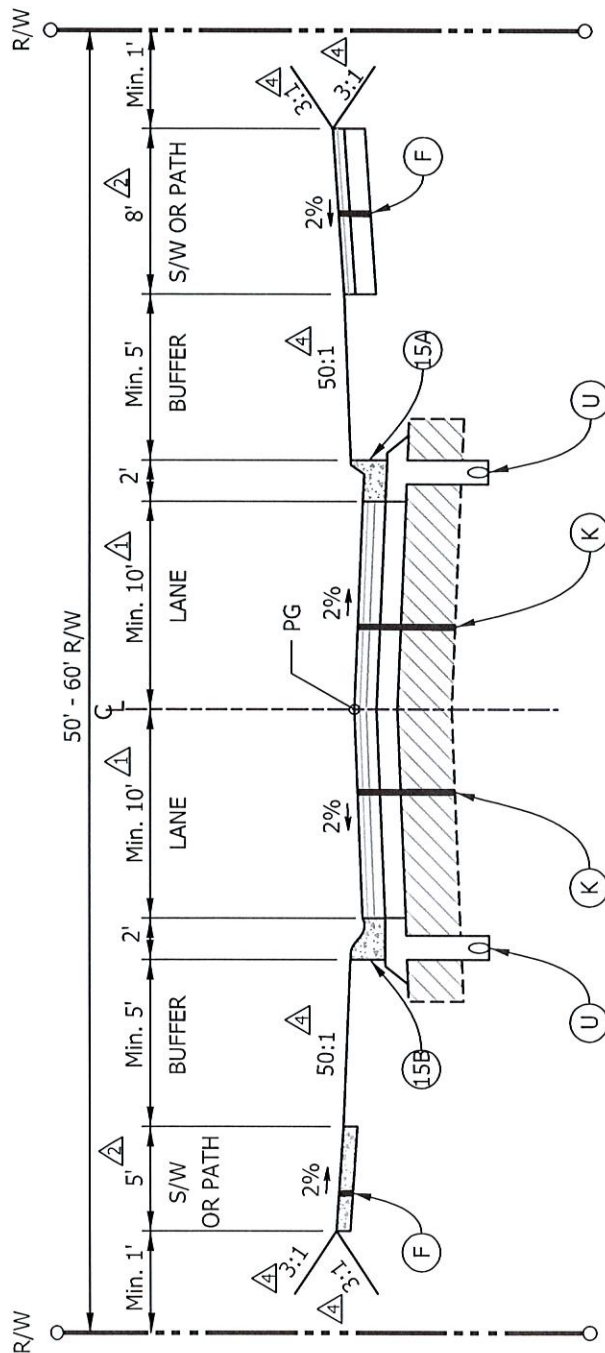
**BOONE COUNTY HIGHWAY DEPARTMENT**

**TYPICAL RURAL LOCAL STREET/RESIDENTIAL FEEDER  
CROSS SECTION**



STANDARD DRAWING
NO. BC-400-007
SHEET 1 OF 1
SCALE: NO SCALE





**LEGEND:**

**NOTES:**

- (F) Concrete Sidewalk - See STD Drawing BC-602-001 Or HMA Path - See STD Drawing BC-602-002
- (K) Pavement Layer
- (U) Underdrain (Typ.) - See STD Drawing BC-401-001
- (15A) Mountable Concrete Curb And Gutter (Typ.) - See STD Drawing BC-601-001 Local Streets Only
- (15B) Rolled Curb (Typ.) - See STD Drawing BC-601-001 Subdivision Only

- △ See Boone County Thoroughfare Plan; 2 Lane Roadway - 1 x 10' Lane Each Direction (Min.) Addition Of Optional Bike Lane - 4' Addition Of Optional Parking Lane - 8' (Min.)
- △ See Boone County Thoroughfare Plan; Both Sides Required 5' Sidewalk Or 8' - 12' Multi-use Path
- △ Tack Coat To Be Applied Between Pavement Lifts And On All Vertical Surfaces
- △ Foreslopes And Backslopes Shall Be In Accordance With Clear Zone Requirements Set Forth By INDOT Design Manual (Current Edition)

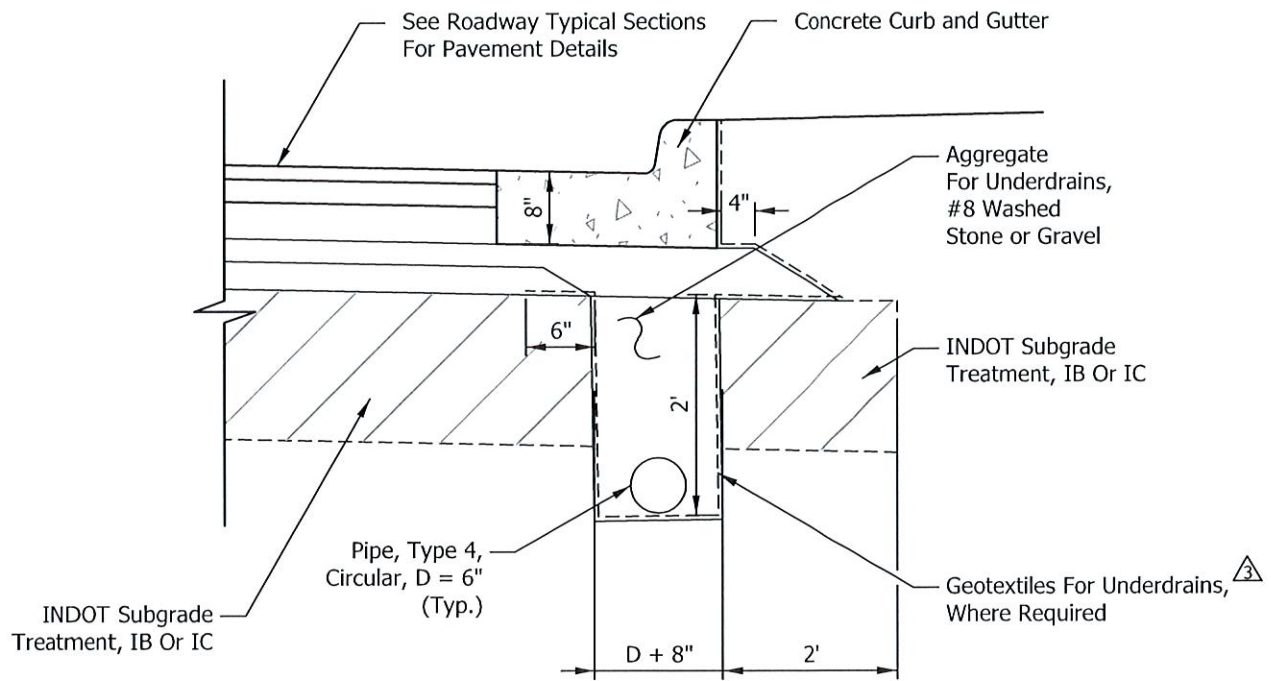
Pavement Layer	Local Streets	Residential Feeder
HMA Surface, 9.5mm, Type B	1.5"	1.5"
HMA Intermediate, 12.5mm, Type B	2.0"	2.5"
HMA Base, 19.0mm, Type B	3.0"	2.5"
HMA Intermediate OG, 19.0mm, Type B	-	2.0"
Compacted Aggregate No. 53	6.0"	8.0"
INDOT Subgrade Treatment, IB or IC	Yes	Yes

**BOONE COUNTY HIGHWAY DEPARTMENT**

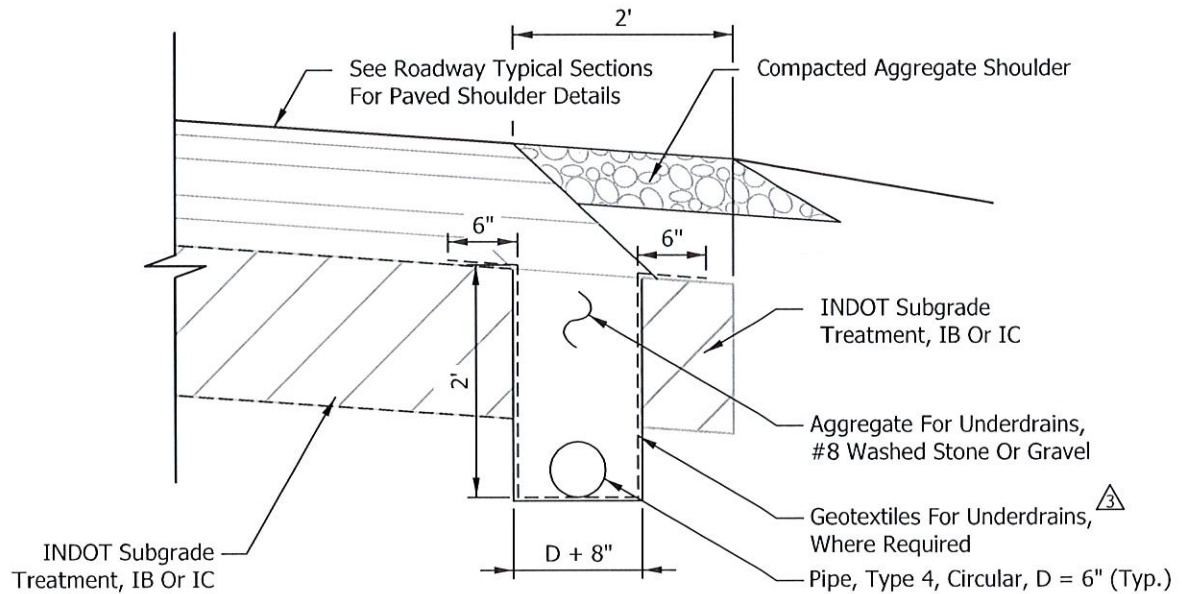
**TYPICAL URBAN LOCAL STREET/RESIDENTIAL FEEDER CROSS SECTION**



STANDARD DRAWING
NO. BC-400-008
SHEET 1 OF 1
SCALE: NO SCALE



CURB AND GUTTER UNDERDRAIN DETAIL



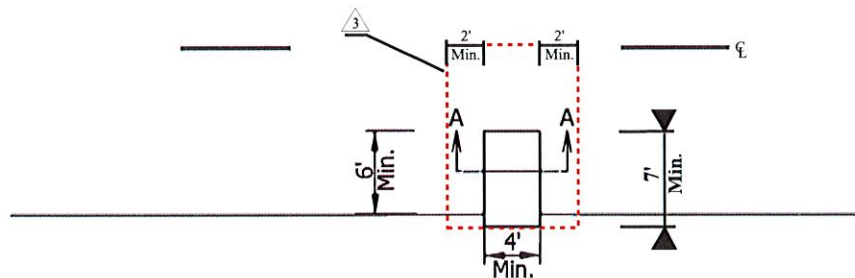
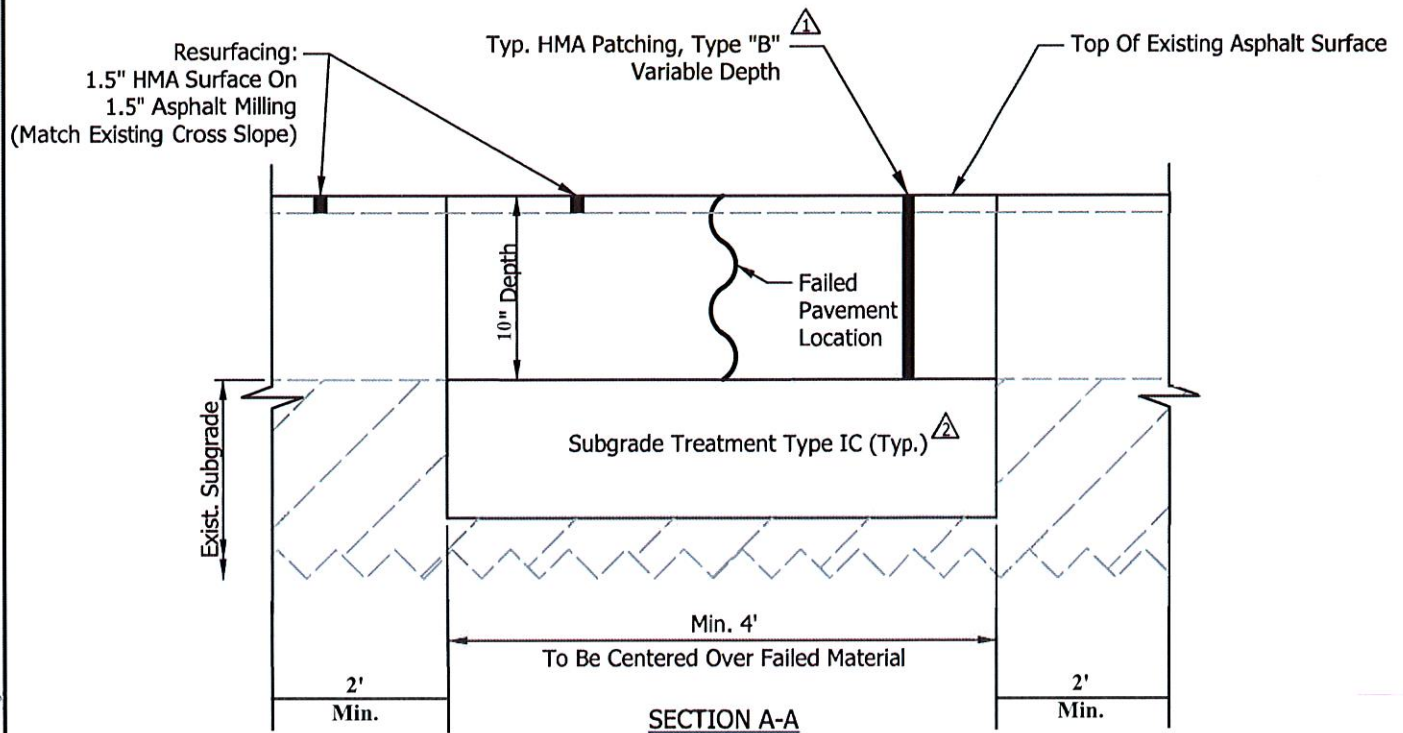
PAVED SHOULDER UNDERDRAIN DETAIL

NOTES:

1. Underdrain Outlets To Be Connected To Existing/Proposed Storm System Or Roadside Ditches As Approved By Boone County Highway Engineer
2. No Blind Taps Into Storm System Permitted

△ Per INDOT Standard Specifications

**BOONE COUNTY HIGHWAY DEPARTMENT**



- △ Typ. HMA Patching To Consist Of:  
10.0" HMA Base, 25.0mm, Type B  
Mill And Remove Existing Pavement - Clean Break Required  
Place Patch Prior To Milling And Resurfacing
- △ INDOT Subgrade Treatment Type IC May Be Substituted With The Following; Excavate And Replace 12" Of The Subgrade With Flowable - Fill may Substitute INDOT Subgrade Treatment Type III As Approved By Boone County Highway Engineer.
- △ Areas To Be Milled And Resurfaced After Base Repair On Alternate 1 A Through D Shall Consist Of Once (1) Foot Paved Shoulders, Resurface Limits To Centerline Of Roadway, And Two (2) Feet Longer In Either Direction Of Base Repair Limits.

Revised May of 2024

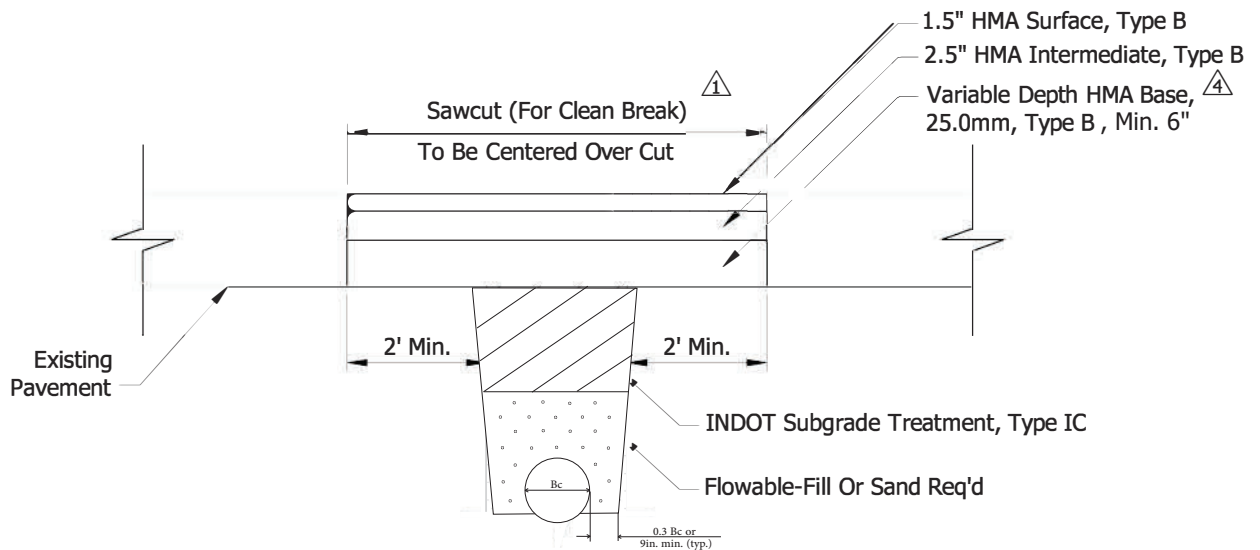
## BOONE COUNTY HIGHWAY DEPARTMENT



### TYPICAL FULL DEPTH PATCH DETAIL

STANDARD DRAWING
NO. BC-402-001
SHEET 1 OF 1
SCALE: NO SCALE

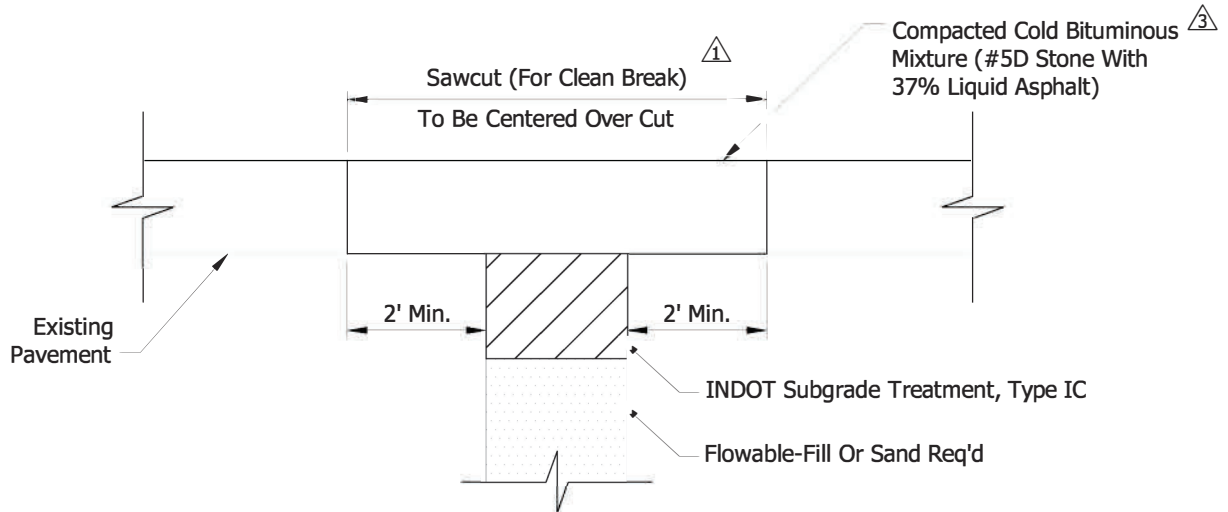




### PERMANENT PATCH

#### NOTES:

1. Saw Cut And Remove Existing Pavement - Clean Break Required and Shall be Perpendicular to the Roadway
2. Contractor Shall Mill (1.5") Existing Pavement 10' In Each Direction From Trench Centerline or 2' Wider Than Excavation Limit, Whichever Is Greater, And From Edge Of Pavement To Centerline Of Pavement Or From Edge Of Pavement To Edge Of Pavement If Road Cut Extends Past Centerline Of Pavement, Then Replace With 1.5" HMA Surface, 9.5mm, And Appropriate Pavement Markings.
3. The Existing Vertical Edge Of Pavement Is To Be Tack Coated Prior To The Placement Of New Asphalt.
4. Base Layer Thickness Determined By Roadway Classification Or Existing Pavement Thickness, But Shall Be Minimum 6".



### TEMPORARY PATCH (FROM NOV. 10 - APRIL 15)

#### NOTES:

1. Saw Cut And Remove Existing Pavement - Clean Break Required and Shall be Perpendicular to the Roadway
2. The Existing Vertical Edge Of Pavement Is To Be Tack Coated Prior To The Placement Of New Asphalt.
3. Thickness Determined By Roadway Classification Or Existing Pavement Thickness.

REVISED JUNE 2025

## BOONE COUNTY HIGHWAY DEPARTMENT



### TYPICAL HMA ROAD CUT PATCH DETAIL

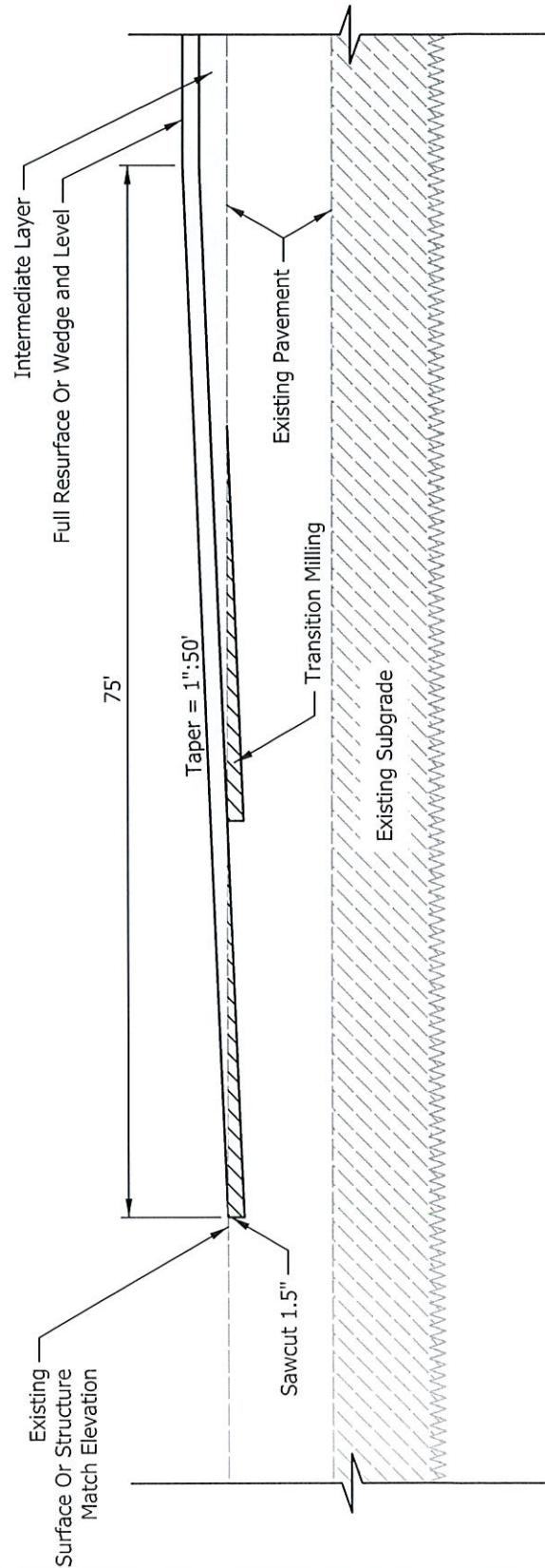
STANDARD DRAWING

NO. BC-402-002

SHEET 1 OF 1

SCALE: NO SCALE





## BOONE COUNTY HIGHWAY DEPARTMENT

### TYPICAL TERMINUS TRANSITION

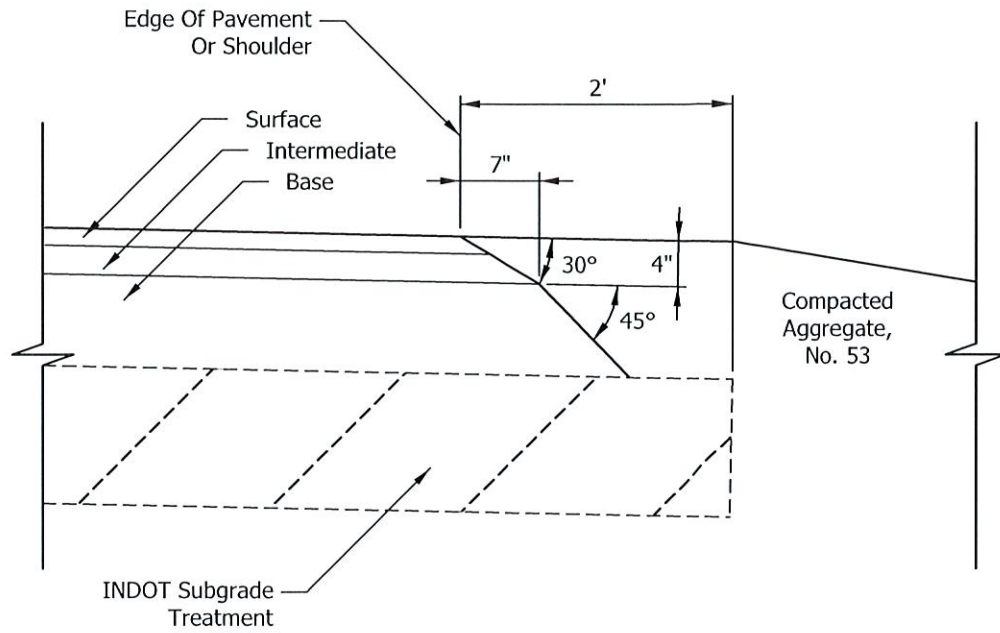


STANDARD DRAWING

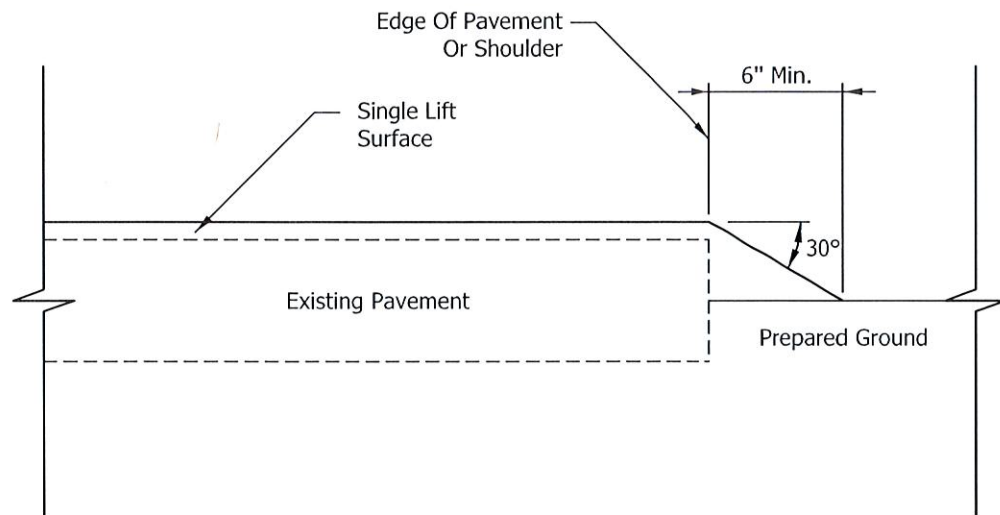
NO. BC-403-001

SHEET 1 OF 1

SCALE: NO SCALE



HMA SAFETY EDGE



SINGLE LIFT SURFACE SAFETY EDGE

BOONE COUNTY HIGHWAY DEPARTMENT



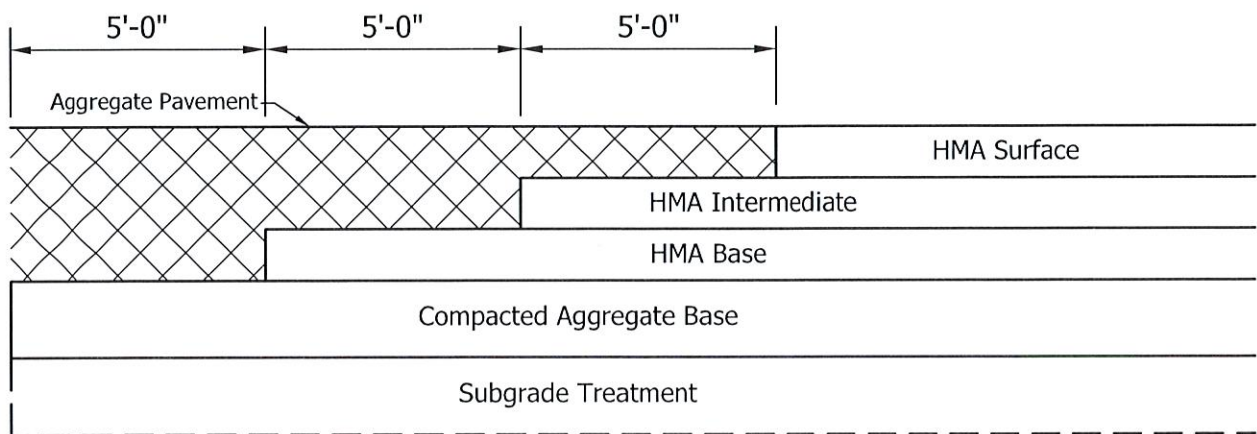
TYPICAL SAFETY EDGE DETAILS

STANDARD DRAWING

NO. BC-403-002

SHEET 1 OF 1

SCALE: NO SCALE



REVISED MAY 2024

BOONE COUNTY HIGHWAY DEPARTMENT

AGGREGATE TO HMA PAVEMENT TRANSITION DETAIL



STANDARD DRAWING  
BC-403-003  
SHEET 1 OF 1  
SCALE: NO SCALE



INDEX OF BOONE COUNTY STANDARDS  
500 SERIES STANDARD DRAWINGS  
CONCRETE PAVEMENTS

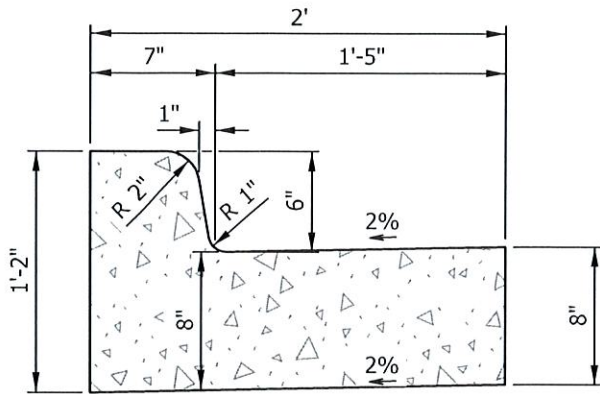
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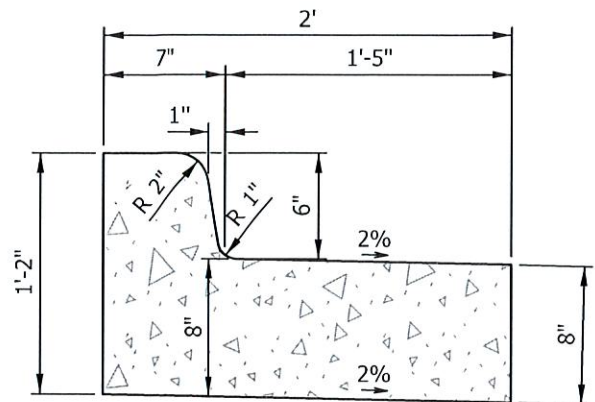
# INDEX OF BOONE COUNTY STANDARDS 600 SERIES STANDARD DRAWINGS INCIDENTAL CONSTRUCTION

Typical Concrete Curb and Gutter and Concrete Curb .....	BC-601-001
Typical Concrete Sidewalk Detail .....	BC-602-001
Typical HMA Multi-Use Path .....	BC-602-002
Typical Subdivision Residential Drive .....	BC-603-001
Typical Urban Commercial Drive .....	BC-603-002
Typical Field Entrance .....	BC-603-003
Typical Rural Residential Drive .....	BC-603-004
Typical Rural Commercial Drive .....	BC-603-005
Typical Subdivision Entrance .....	BC-603-006
Typical Street Approach .....	BC-603-007
Typical Acceleration/Deceleration Lane .....	BC-603-008
Typical Mailbox Approach .....	BC-603-009
Typical Culvert Detail With Open Ditches .....	BC-603-010
Typical Cul-De-Sac .....	BC-604-001
Typical Temporary Cul-De-Sac .....	BC-604-002

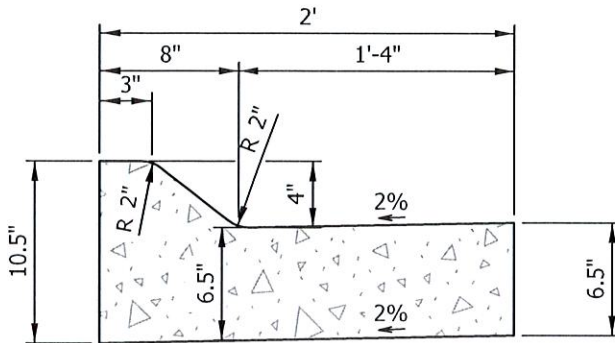




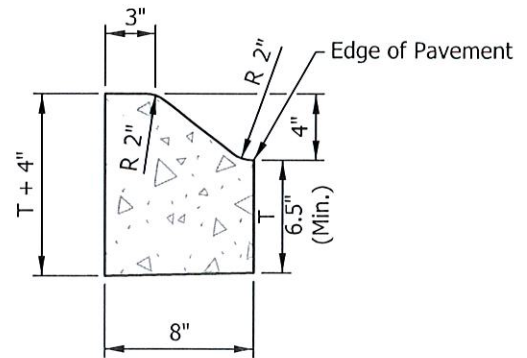
CONCRETE CURB AND GUTTER  
STANDARD (Typ.)



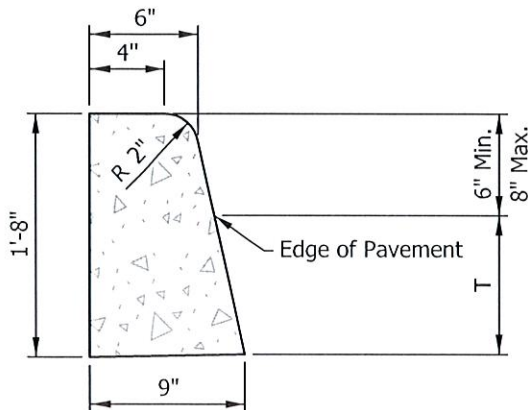
CONCRETE CURB AND GUTTER  
MODIFIED (Typ.)



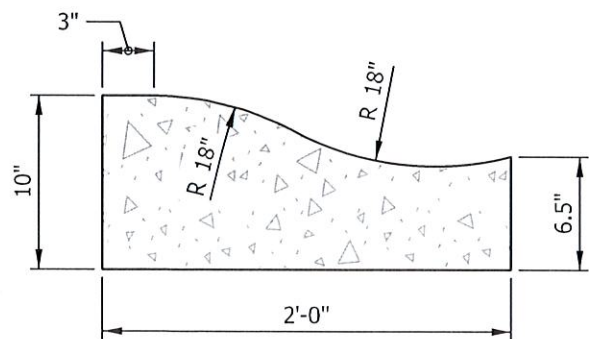
CONCRETE CURB AND GUTTER  
MOUNTABLE (Typ.)



CONCRETE CURB  
MOUNTABLE



CONCRETE CURB  
BARRIER



ROLLED CURB

NOTES

T = Nominal Pavement Thickness

**BOONE COUNTY HIGHWAY DEPARTMENT**

**CONCRETE CURB AND GUTTER  
AND CURB**



STANDARD DRAWING

NO. BC-601-001

SHEET 1 OF 1

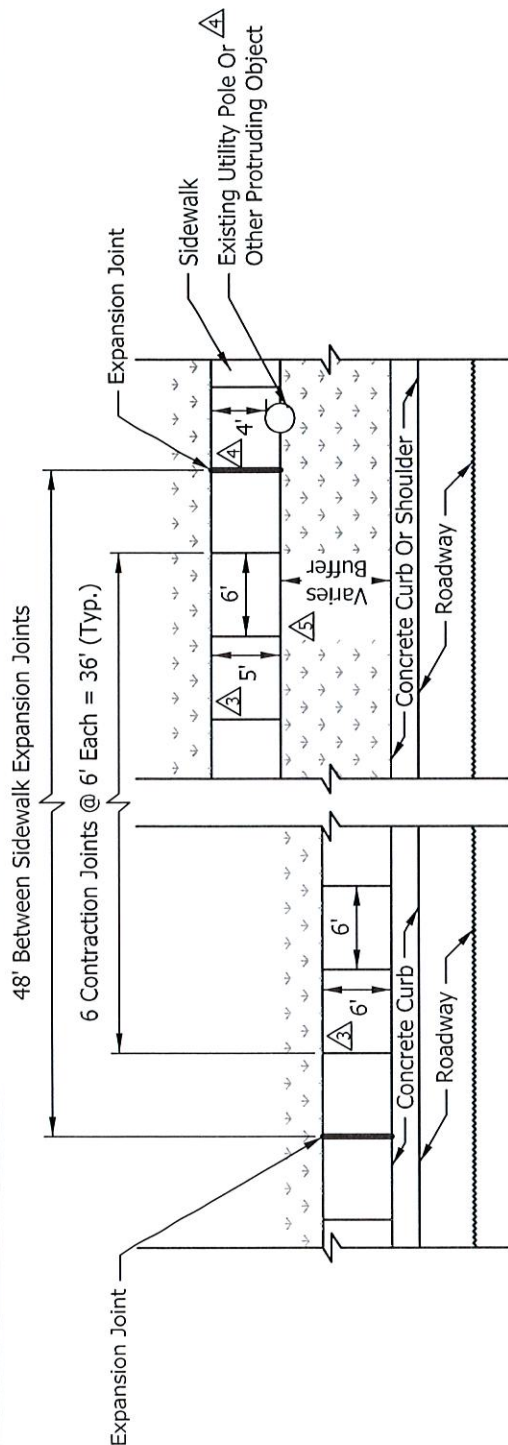
SCALE: NO SCALE



## BOONE COUNTY HIGHWAY DEPARTMENT

## TYPICAL CONCRETE SIDEWALK

STANDARD DRAWING
NO. BC-602-001
SHEET 1 OF 1
SCALE: NO SCALE



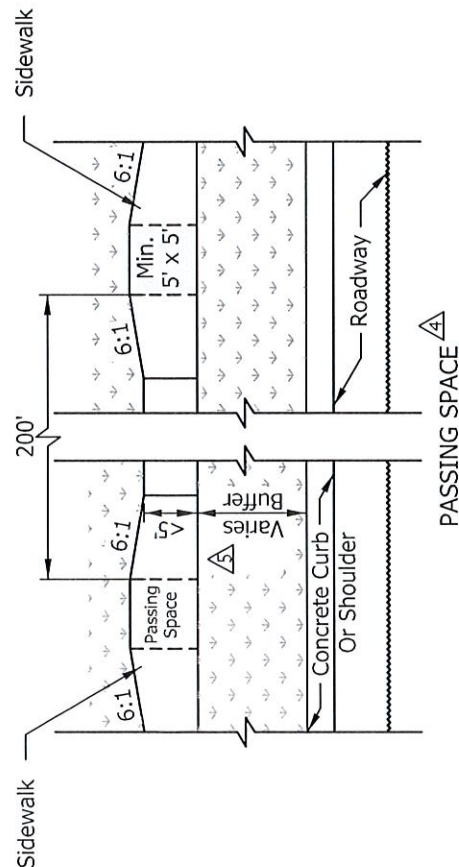
SIDEWALK PLAN VIEW

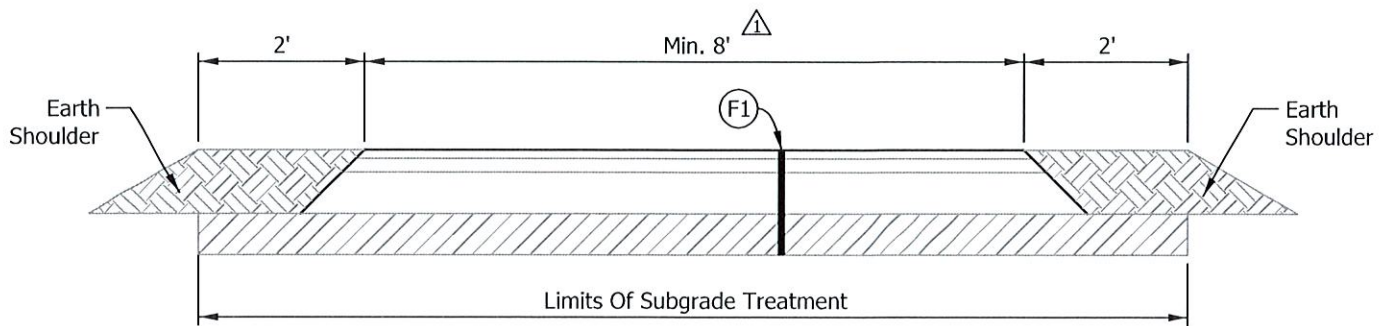
NOTES:

1. All Slopes Are Absolute Rather Than Relative To The Sidewalk Or Roadway Grade And Are Given As Maximums. Slopes At Least 0.50% Less Than The Maximum Are Preferred.
2. The Grade Of The Sidewalk Is Measured In The Direction Of Pedestrian Travel And Shall Not Exceed The Grade Of The Adjacent Roadway. The Cross Slope Is Measured Perpendicular To The Direction Of Pedestrian Travel And Shall Not Exceed 2.00%
  - ④ Where There Is A Buffer Between The Sidewalk And The Curb, The Preferred Minimum Sidewalk Clear Width Is 5'. Where There Is No Buffer Between The Sidewalk And The Curb, The Preferred Minimum Sidewalk Width Is 6'.
  - ④ A 4' Minimum Clear Width Shall Be Provided Adjacent To Street Furniture, Mailbox, Utility Pole, Or Other Protruding Object. Where The Sidewalk Clear Width Is Less Than 5', A Passing Space With Dimensions Of 5' X 5' Shall Be Provided At 200' Intervals
  - ④ See Roadway Typical Sections for Dimensions

**LEGEND:**

- (F) 4" Sidewalk, Concrete On  
4" Compacted Aggregate No. 53





#### LEGEND:

- (F1) 1.5" HMA Surface, 9.5mm On  
 2.5" HMA Intermediate, 19.0mm On  
 6.0" Compacted Aggregate, No. 53 On  
 Typ. INDOT Subgrade Treatment Type III

#### NOTES:

- △ Width Varies, Typ. 8' - 12'  
 Cross Slope Varies, Typ. Max. 2.0%

### BOONE COUNTY HIGHWAY DEPARTMENT



### TYPICAL HMA MULTI-USE PATH

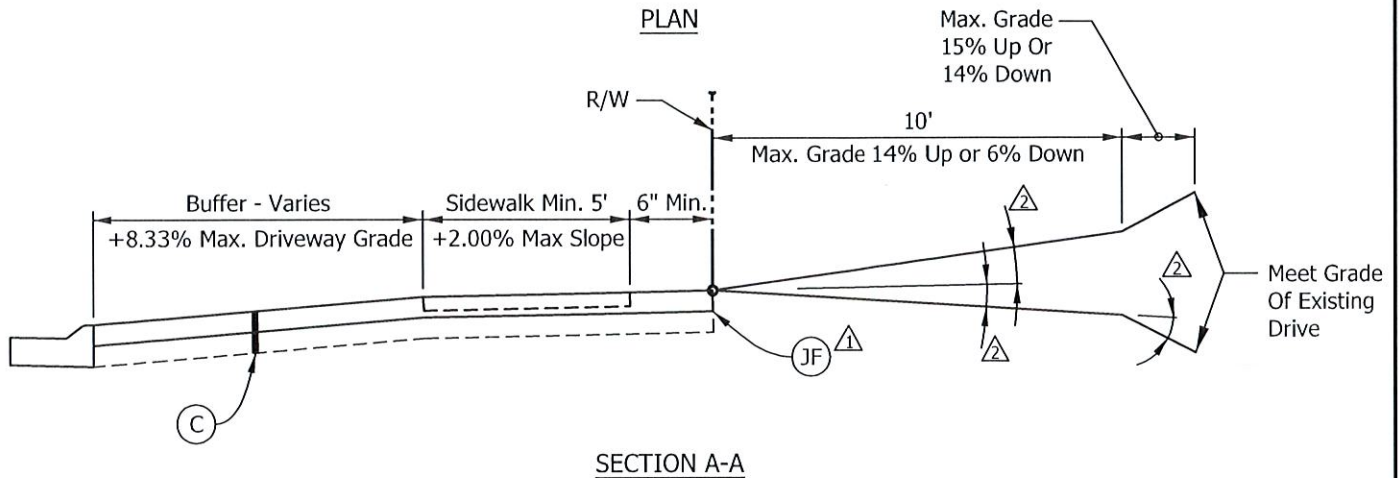
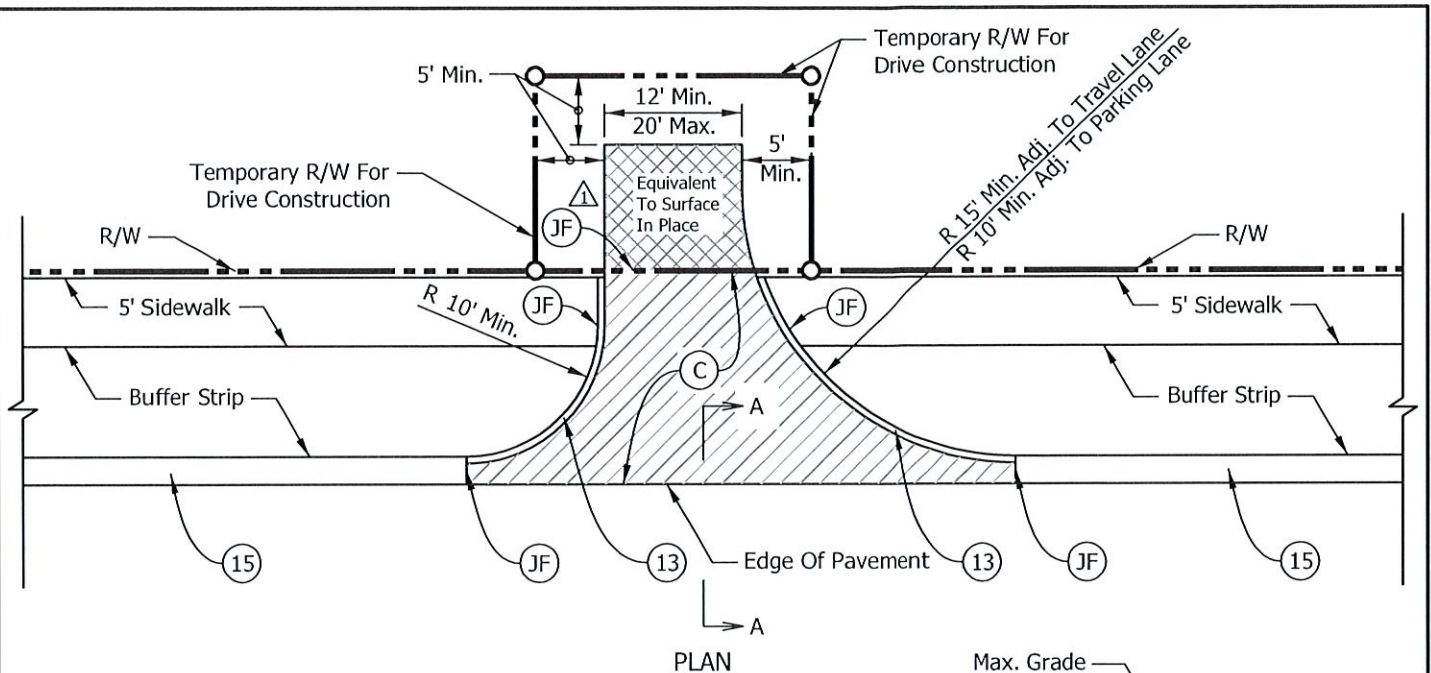
STANDARD DRAWING

NO. BC-602-002

SHEET 1 OF 1

SCALE: NO SCALE

3/27/2019 7:24:03 AM - Boone County As-Needed Assistance Standards Consolidation\09Road\04 Design Files\02 DGN\Plans\603-001\_BC\_Res\_Drive\_Sht\_Typical.dgn



#### LEGEND:

(C)	Pavement Layer	Asphalt	Concrete	Aggregate
	HMA Surface, Type B	-	-	-
	HMA Intermediate, Type B	-	-	-
	HMA Base, Type B	-	-	-
	Compacted Aggregate No. 53	-	-	-
	INDOT Subgrade Treatment	-	Type II	8" (3)
	PCCP for Approaches	N/A	6"	N/A

- (JF) 1/2" Preformed Joint Filler
- (13) Monolithic Curb See STD Drawing BC-601-001
- (15) Concrete Curb And Gutter See STD Drawing BC-601-001

#### NOTES:

- (1) Joint Filler Required When Existing Drive Is PCCP
- (2) Max. Algebraic Diff. In Grades;  
Crests 8.00%  
Sags 12.00%
- (3) 8" Compacted Aggregate May Be Substituted For PCCP With Approval From County Engineer
- 4. Sidewalk Elevation Transitions At Drives To Be In Compliance With Current ADA Guidelines
- 5. Driveway Culverts Which Cannot Be located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards
- 6. For Drainage Structure Details See STD Drawing BC-603-010

## BOONE COUNTY HIGHWAY DEPARTMENT

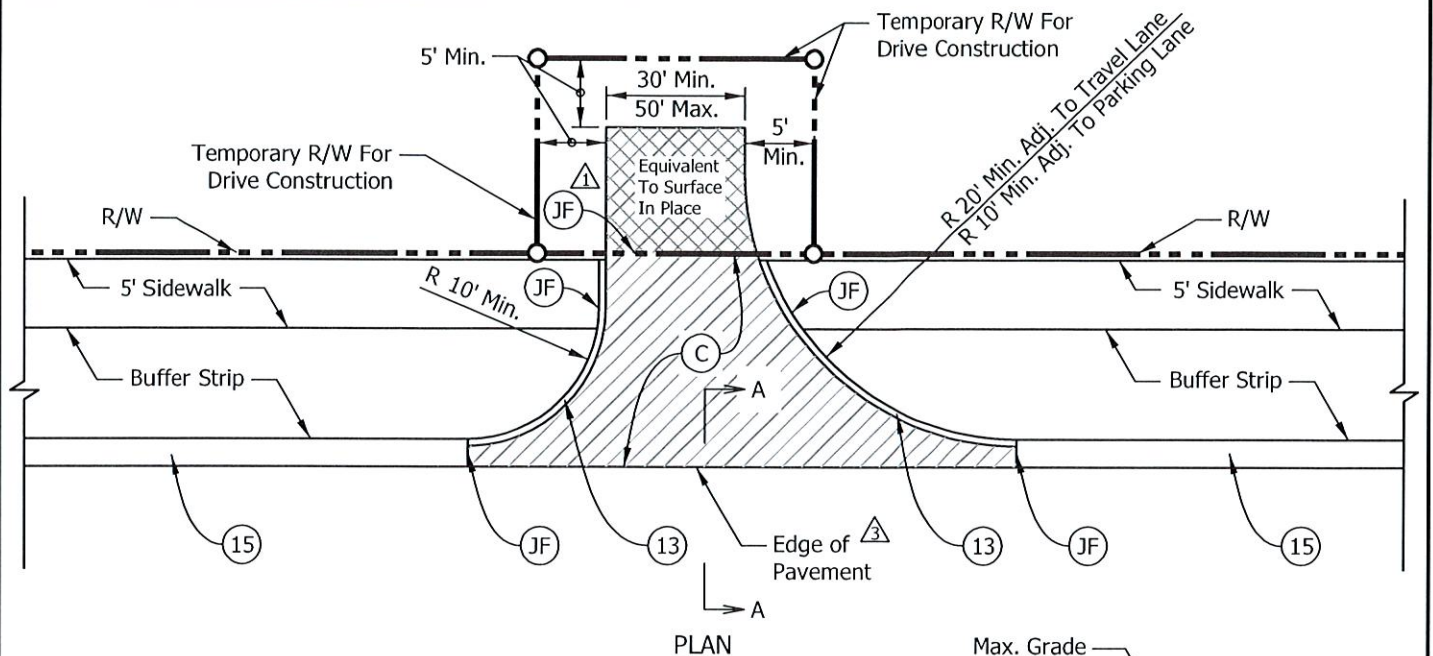
### TYPICAL SUBDIVISION RESIDENTIAL DRIVEWAY



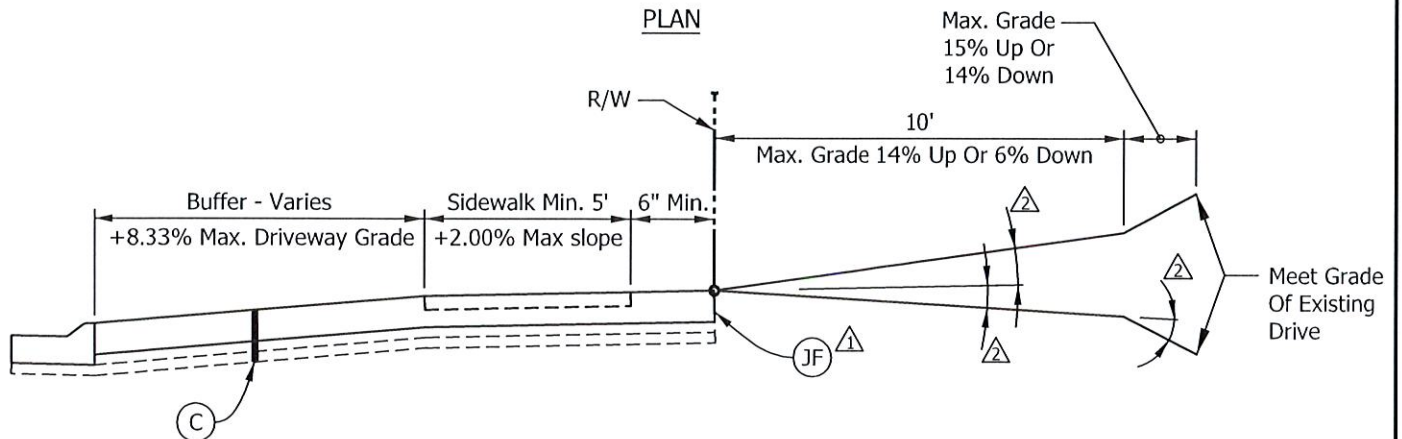
STANDARD DRAWING
NO. BC-603-001
SHEET 1 OF 1
SCALE: NO SCALE



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PLAN



LEGEND:

SECTION A-A

NOTES:

C	Pavement Layer	Asphalt	Concrete	Aggregate
	HMA Surface, Type B	-	-	-
	HMA Intermediate, Type B	-	-	-
	HMA Base, Type B	-	-	-
	Compacted Aggregate No. 53	-	-	-
	INDOT Subgrade Treatment	-	Type II	-
	PCCP for Approaches	N/A	9"	N/A

- (JF) 1/2" Preformed Joint Filler
- (13) Monolithic Curb See STD Drawing BC-601-001
- (15) Concrete Curb And Gutter See STD Drawing BC-601-001

- △ Joint Filler Required When Existing Drive Is PCCP
- △ Max. Algebraic Diff. In Grades; Crests 8.00% Sags 12.00%
- △ If Warranted By County Engineer Acceleration/Deceleration Taper To Be Added, See STD Drawing BC-603-008
- 4. Sidewalk Elevation Transitions At Drives To Be In Compliance With Current ADA Guidelines
- 5. Driveway Culverts Which Cannot Be located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards
- 6. Commercial Drives Shall Be Required At Truck Stops And Entrances To Heavy Industrial Properties
- 7. For Drainage Structure Details See STD Drawing BC-603-010

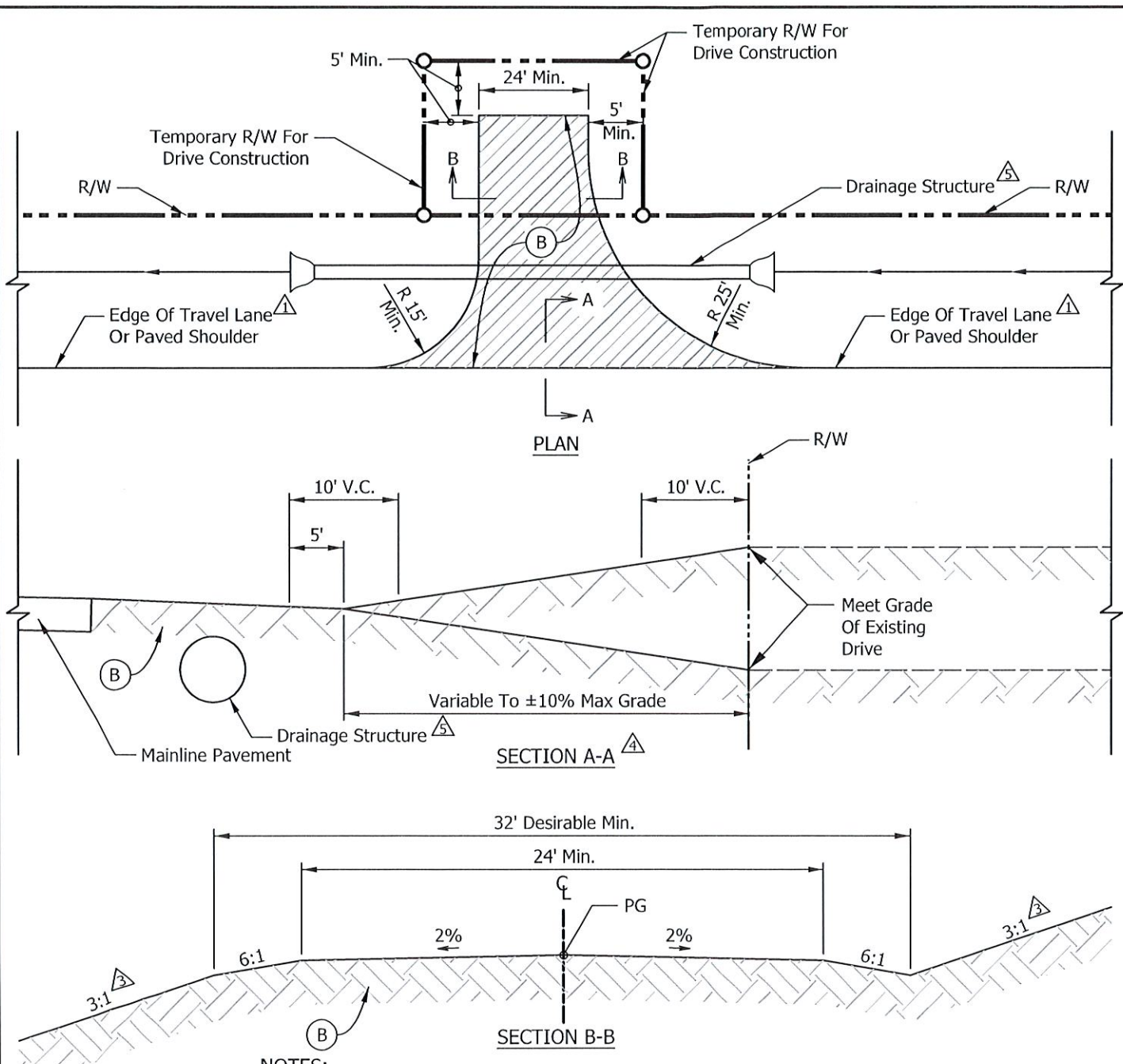
BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL URBAN COMMERCIAL DRIVEWAY



STANDARD DRAWING
NO. BC-603-002
SHEET 1 OF 1
SCALE: NO SCALE

3/27/2019 7:34:04 AM I:\1720004 - Boone County As-Needed Assistance\Standards Consolidation\09Road\04 Design files\02 DCN\Plans\600s\603-003\_Field Drive\_Sht\_Typical.dgn



#### NOTES:

1. Where The Shoulder Is Earth Or Aggregate, Or When The Paved Shoulder Is Less Than 8', The Drive Radii Shall Be Tangent To The Travel Lane. Where The Paved Shoulder Is 8' Or More, The Drive Radii Shall Be Tangent To The Paved Shoulder
2. Driveway Culverts Which Cannot Be located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards
3. Foreslopes And Backslopes Shall Be In Accordance With Clear Zone Requirements Set Forth By INDOT Design Manual (Current Edition)
4. Minimum 2% Grade For 10' Away From Edge Of Pavement Required For Gravel Drives
5. For Drainage Structure Details - See STD Drawing BC-603-010

#### LEGEND:

(B) Earth Fill Or Aggregate As Needed

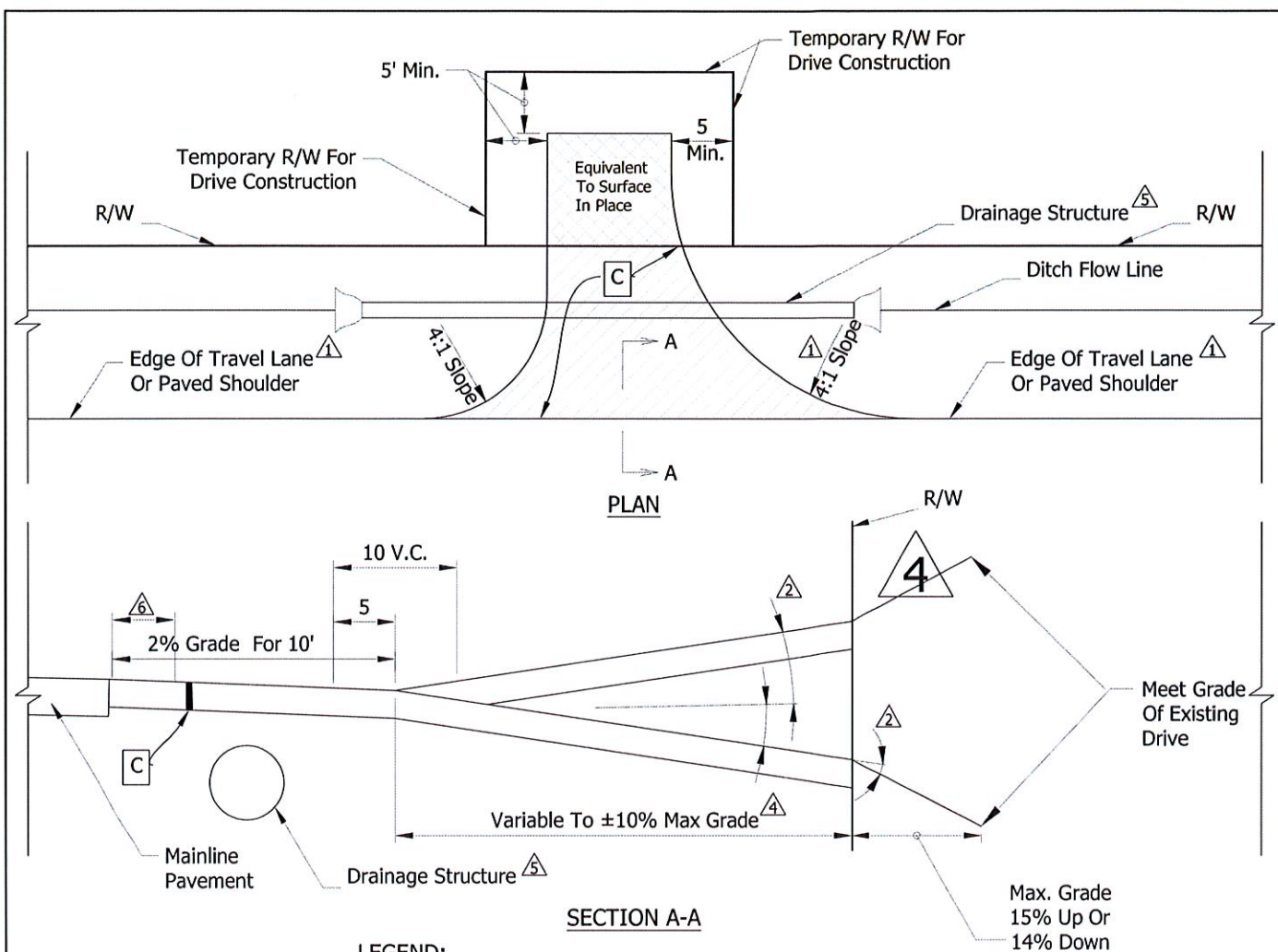
### BOONE COUNTY HIGHWAY DEPARTMENT



### TYPICAL FIELD ENTRANCE

STANDARD DRAWING  
NO. BC-603-003  
SHEET 1 OF 1  
SCALE: NO SCALE





## BOONE COUNTY HIGHWAY DEPARTMENT

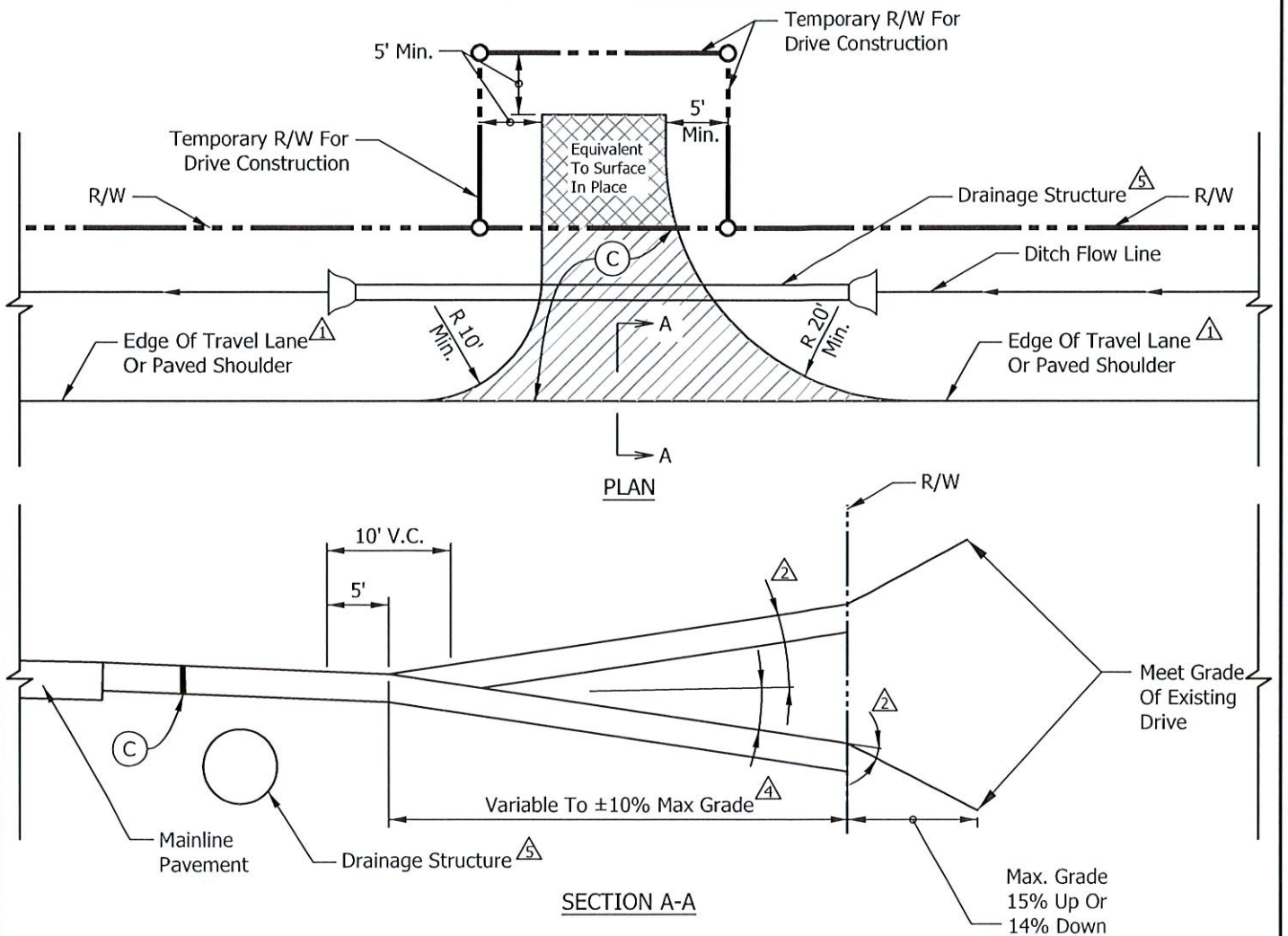
### TYPICAL RURAL RESIDENTIAL DRIVEWAY

STANDARD DRAWING  
NO. BC-603-004  
SHEET 1 OF 1  
SCALE: NO SCALE

REVISED MAY 2021



3/27/2019 7:24:05 AM I:\P\170004 - Boone County As-Needed Assistance\Standards Consolidation\09Road\04 Design files\02 DGN\Plans\6003-005\_Rural Commercial Drive\_Sht\_Typical.dgn



#### LEGEND:

C	Pavement Layer	Asphalt	Concrete	Aggregate
	HMA Surface, Type B	1.5"	-	-
	HMA Intermediate, Type B	2.5"	-	-
	HMA Base, Type B	8.0"	-	-
	Compacted Aggregate No. 53	-	-	12"
	INDOT Subgrade Treatment	Type II	Type II	Type II
	PCCP for Approaches	N/A	9"	N/A

#### NOTES:

- ① Where The Shoulder Is Earth Or Aggregate, Or When The Paved Shoulder Is Less Than 8', The Drive Radii Shall Be Tangent To The Travel Lane. Where The Paved Shoulder Is 8' Or More, The Drive Radii Shall Be Tangent To The Paved Shoulder
- ② Max. Algebraic Diff. In Grades; Crests 8.00%, Sags 12.00%
3. Driveway Culverts Which Cannot Be located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards
- ④ Minimum 2% Grade For 10' Away From Edge Of Pavement Required For Gravel Drives
- ⑤ For Drainage Structure Details - See STD Drawing BC-603-010

### BOONE COUNTY HIGHWAY DEPARTMENT



### TYPICAL RURAL COMMERCIAL DRIVEWAY

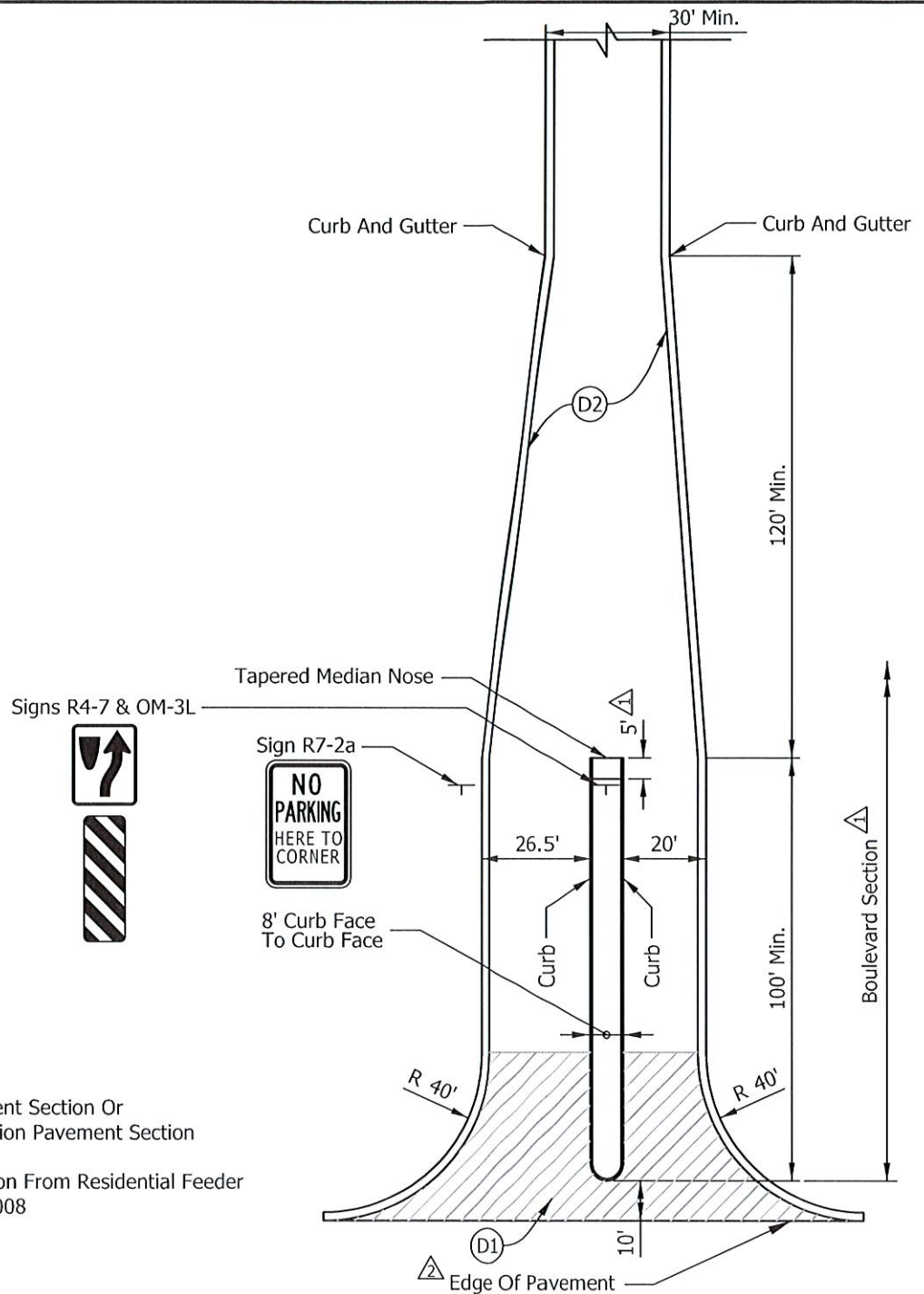
STANDARD DRAWING

NO. BC-603-005

SHEET 1 OF 1

SCALE: NO SCALE

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**LEGEND:**

- (D1) Match Existing Pavement Section Or Acceleration/Deceleration Pavement Section
- (D2) Match Pavement Section From Residential Feeder STD Drawing BC-400-008

**NOTES:**

1. For Boulevard Sections, Continue Median Section; Maximum Length Between Breaks In Medians Shall Be 600' Minimum Length Between Islands Shall Be 30'
2. For Major Subdivisions, Acceleration/Deceleration Taper To Be Used To Tie Into Existing, See STD Drawing BC-603-008
3. Vegetation Shall Not Obstruction Sight Line
4. All Sign Posts Shall Be Mounted On Breakaway Anchors.

**BOONE COUNTY HIGHWAY DEPARTMENT**



**TYPICAL SUBDIVISION ENTRANCE**

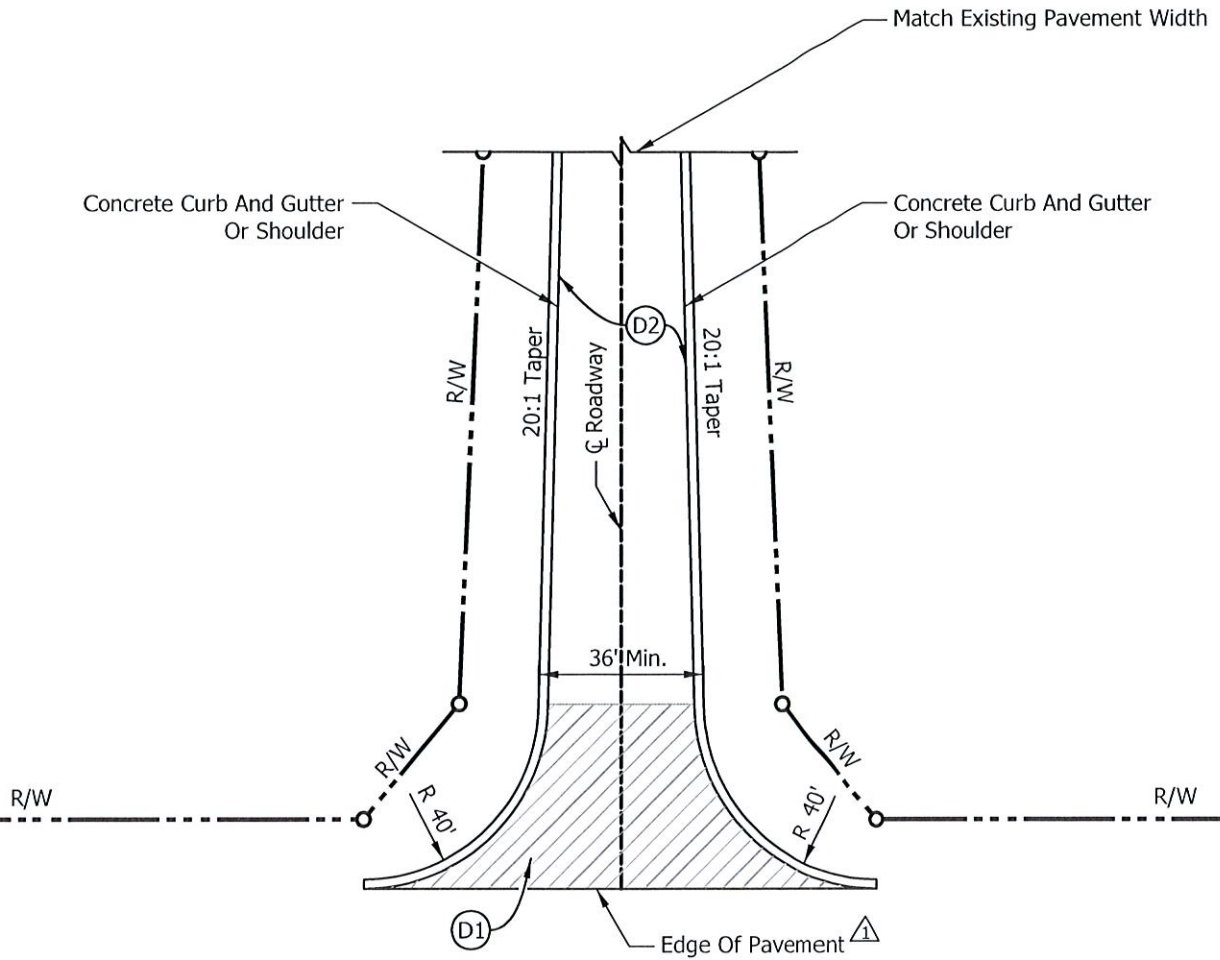
STANDARD DRAWING

NO. BC-603-006

SHEET 1 OF 1

SCALE: NO SCALE

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**LEGEND:**

- (D1) Match Existing Roadway Pavement Section
- (D2) Match Pavement Section From Residential Feeder  
STD Drawing BC-400-008

**NOTES:**

- $\triangle$  Acceleration/Deceleration Taper To Used To Tie Into Existing,  
See STD Drawing BC-603-008

**BOONE COUNTY HIGHWAY DEPARTMENT**

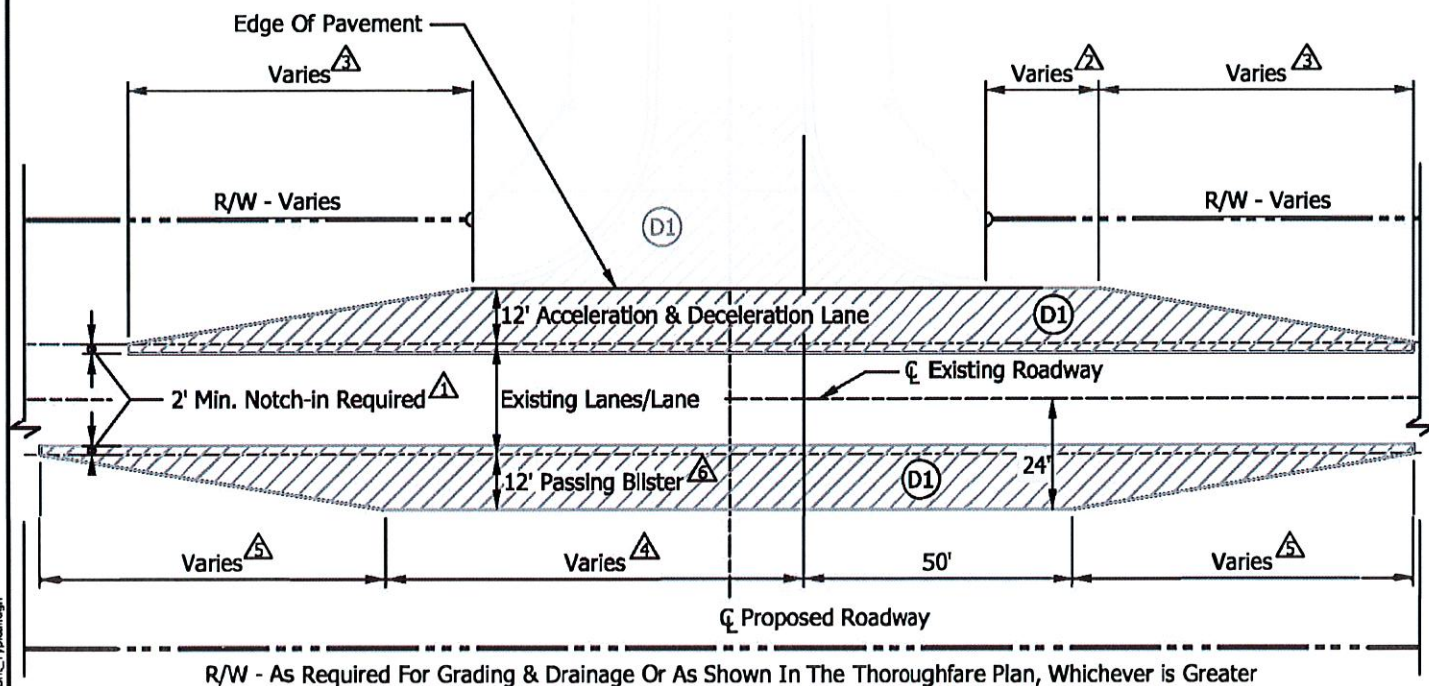


**TYPICAL STREET APPROACH**

STANDARD DRAWING
NO. BC-603-007
SHEET 1 OF 1
SCALE: NO SCALE



3/27/2019 7:24:07 AM I:\P\1170004 - Boone County As-Needed Assistance\Standards Consolidation\09road\04 Design files\02 DGN\Plans\6003-003-BC\_Accel Decel Taper\_Sht\_Typical.dgn



#### NOTES:

- △ Acceleration/Deceleration & Passing Blister To Be Notched Into Existing Pavement With A Minimum Of 2' Clean Saw Cut Edge
- △ Deceleration Tangent See Table
- △ Acceleration/Deceleration Taper See Table
- △ Passing Blister Variable Tangent See Table
- △ Passing Blister Tapers See Table
- △ Passing Blisters Required When Deemed Necessary By The County Engineer
- 7. Construction Plans Shall Include Centerline Profile Of The Existing Road Being Intersected By The Proposed Road. The Profile Shall Extend A Minimum Of 500' From The Entrance Centerline In Each Direction.

Accel And Decel Dimension		
Speed (MPH)	△	△
30 Or Less	75'	125'
Greater Than 30 & Less Than 50	100'	150'
Greater Than 50	100'	200'

Passing Blister Dimension		
Speed (MPH)	△	△
30 Or Less	150'	150'
Greater Than 30 & Less Than 50	150'	200'
Greater Than 50	200'	300'

#### LEGEND:

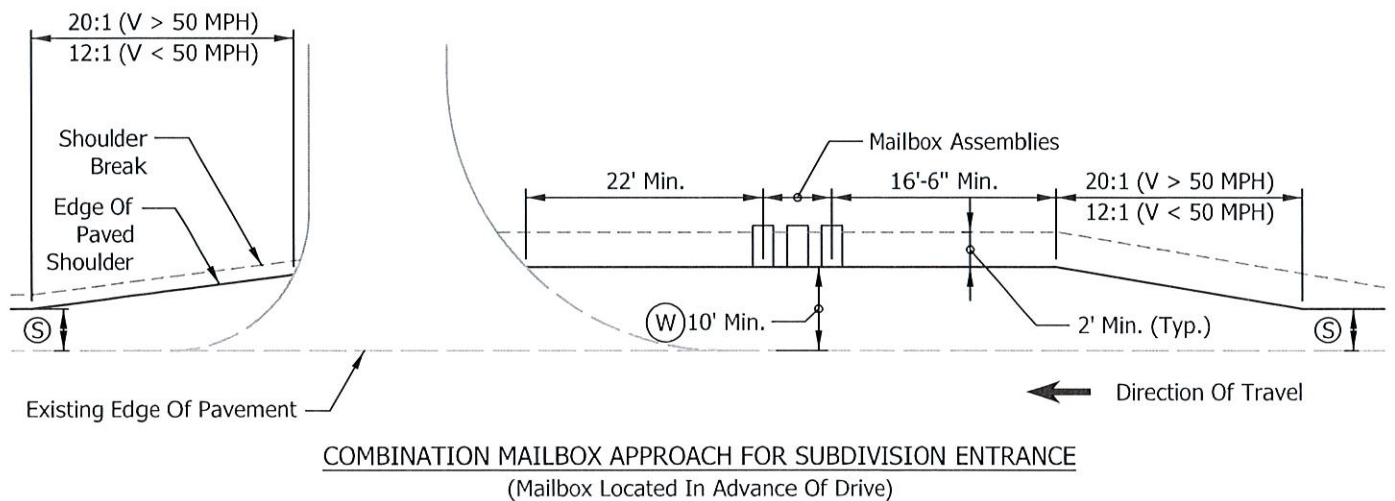
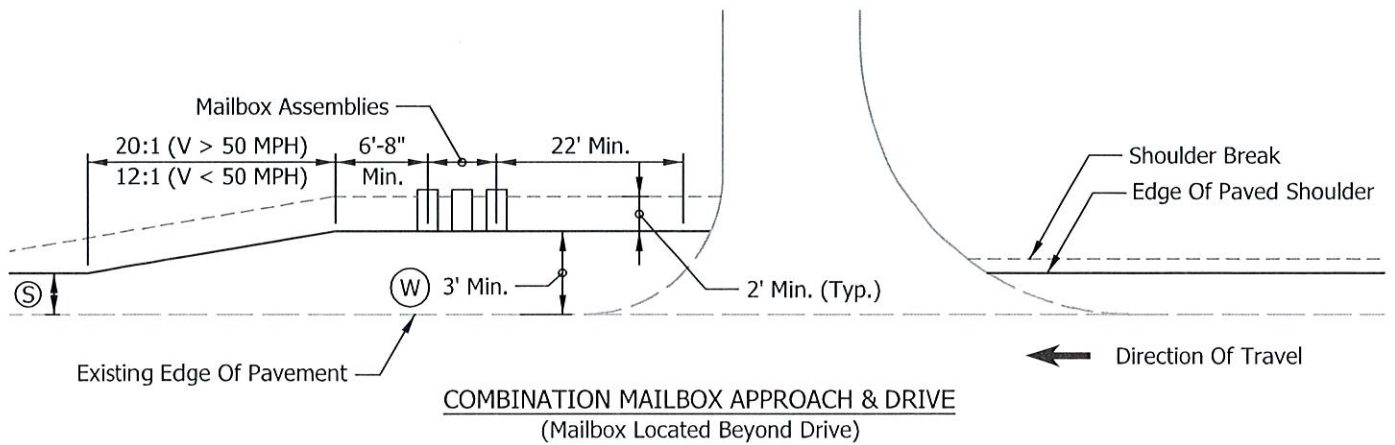
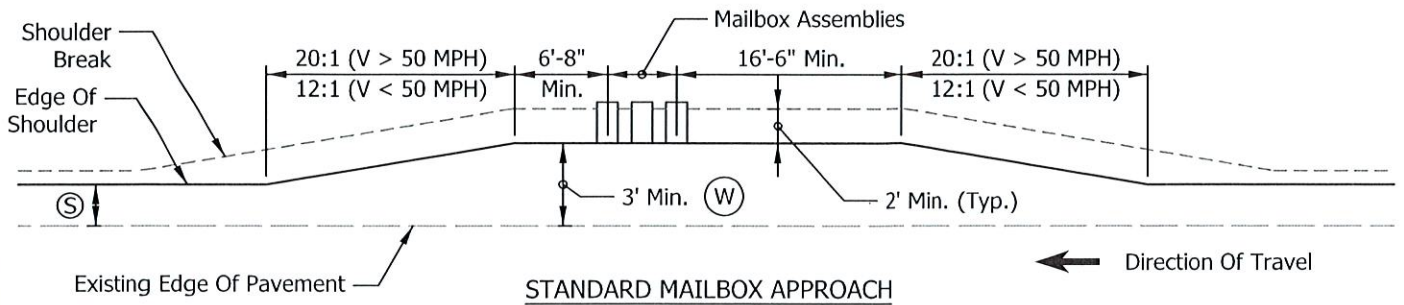
- ⊙ D1 Match Existing Roadway Pavement Section

## BOONE COUNTY HIGHWAY DEPARTMENT



## TYPICAL ACCELERATION / DECELERATION LANE

STANDARD DRAWING  
NO. BC-603-008  
SHEET 1 OF 1  
SCALE: NO SCALE



#### LEGEND:

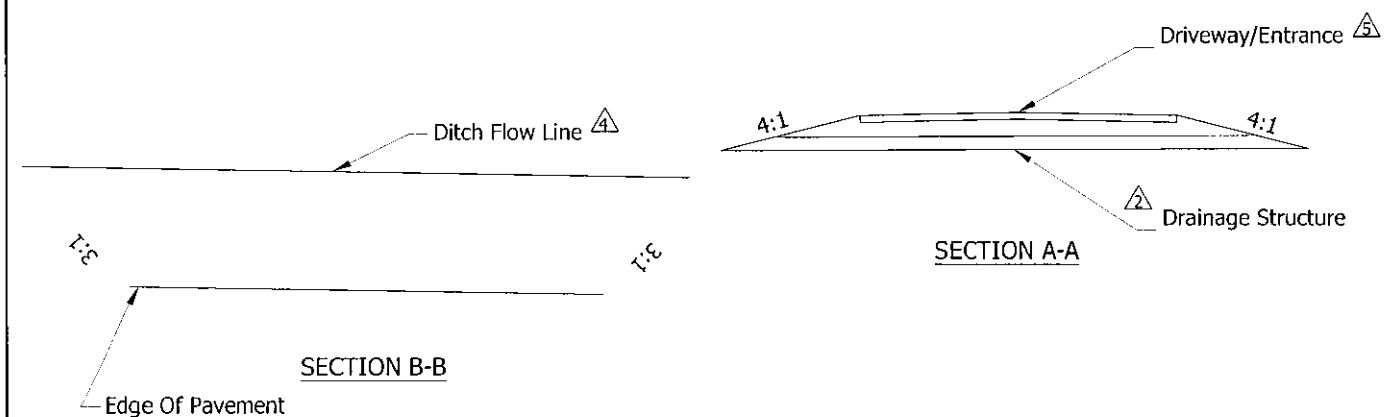
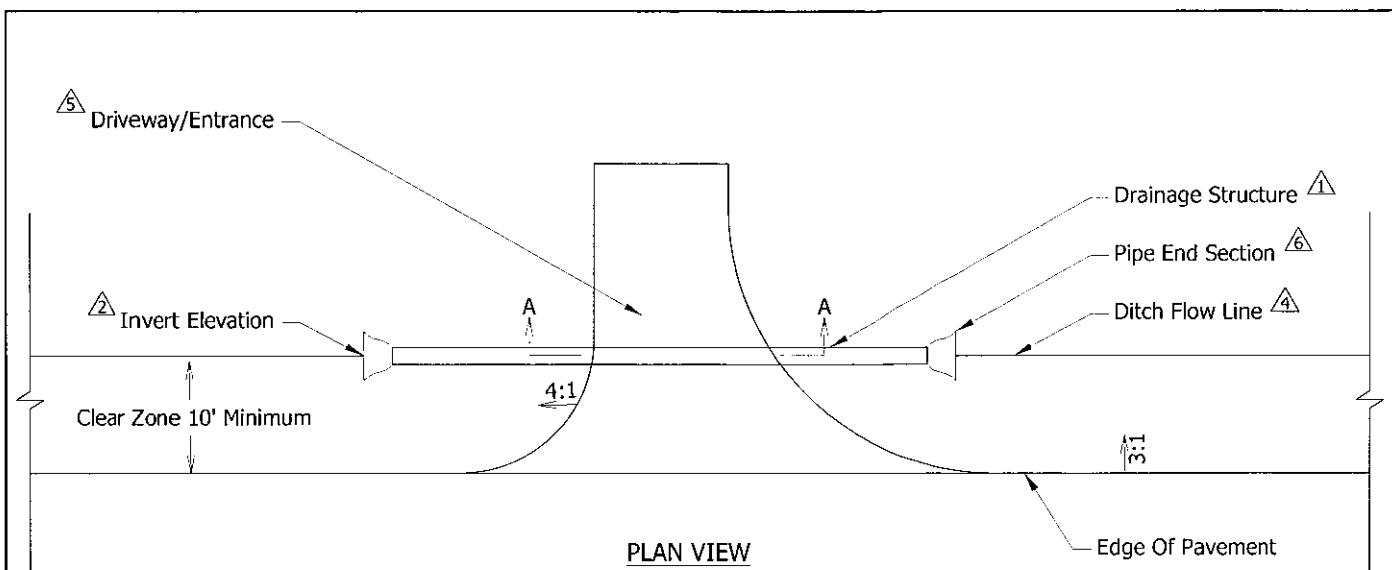
- (W) Match Existing Adjacent Pavement Section (Shoulder Or Lane)
- (S) Shoulder Width From Plans

### BOONE COUNTY HIGHWAY DEPARTMENT



### TYPICAL MAILBOX APPROACH

STANDARD DRAWING
NO. BC-603-009
SHEET 1 OF 1
SCALE: NO SCALE



#### NOTES:

- 1. 12" Min. Diameter Structure (15" Diameter Desirable)  
Or As Directed By Boone County Engineer, Minimum 12" Cover
- 2. Invert Elevations Shall Be Provided At All Downstream And Upstream Inverts.
- 3. Any And All Necessary Ditch And Drainage Improvements Shall Be Provided By Individual Performing The Installation
- 4. Flow Line Elevations Shall Be Provided At The Beginning, End, And Every Grade Break.
- 5. For Subdivision Residential Driveway Details - See STD Drawing BC-603-001  
For Urban Commercial Driveway Details - See STD Drawing BC-603-002  
For Field Entrance Details - See STD Drawing BC-603-003  
For Rural Residential Drive - See STD Drawing BC-603-004
- 6. Pipe End Sections Shall Be In Accordance With Clear Zone Criteria Outlined In Current Version Of AASHTO Roadside Design Guide  
Drainage Structure Should Be A Min of 10' From Edge Of Pavement

REVISED MAY 2021

### BOONE COUNTY HIGHWAY DEPARTMENT

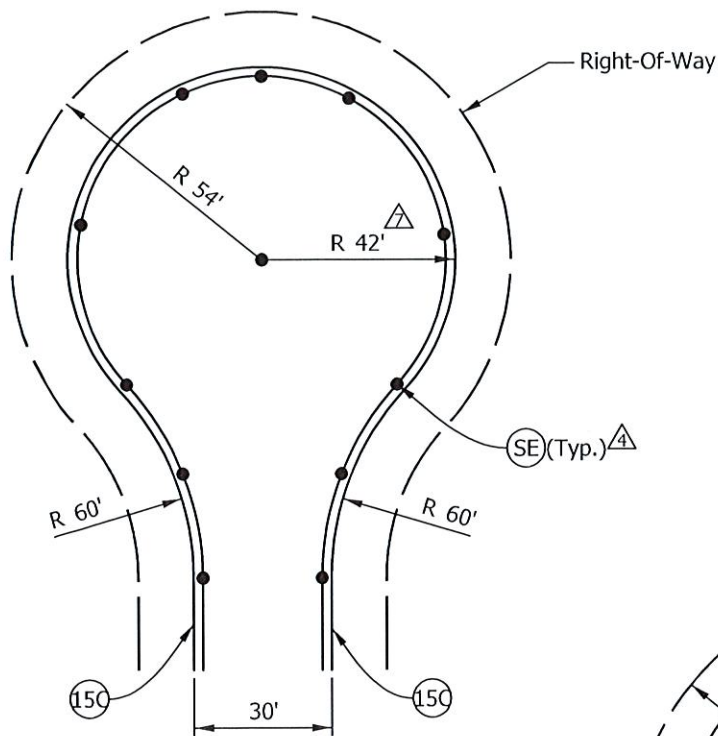
#### TYPICAL CULVERT DETAIL WITH OPEN DITCHES



STANDARD DRAWING  
NO. BC-603-010  
SHEET 1 OF 1  
SCALE: NO SCALE



3/27/2019 7:24:08 AM I:\P\170004 - Boone County As-Needed Assistance\Standards Consolidation\09Road\04 Design files\02 DGN\Plans\604-001\_BC\_Sub Div Cul-De-Sac\_Sht\_Typical.dgn



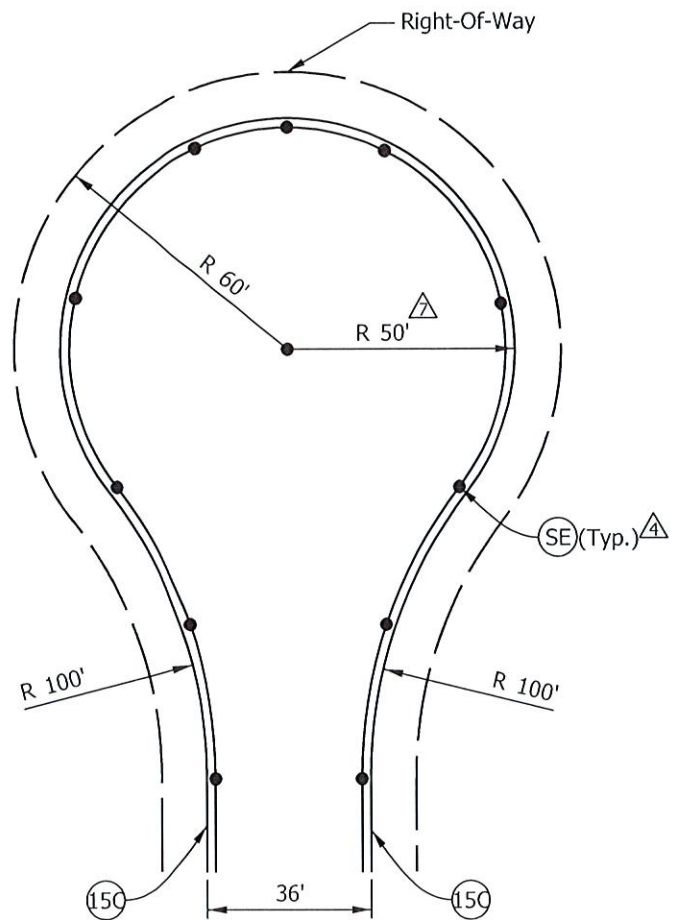
TYPE-1 (CUL-DE-SAC)

**NOTES:**

1. Type-1 Shall Be Used Only In Residential Subdivisions, All Others Shall Be Type-2.
2. Warning Signs Shall Be Posted At The Street Entrance, Indicating No Outlet.
3. One Detail Shall Be Provided For Each Cul-De-Sac. Each Detail Shall Be Identified By Street Name.
- △ Elevations Provided Shall Be Proposed Flow Line Of Gutter.
5. Street Width And Radius Lengths Are Measured To The Back Of Curb.
6. No Center Islands Permitted.
- △ Radius Shall Be In Accordance With Current Fire Protection Ordinance. May Differ From Dimensions Shown.

**LEGEND:**

- (15C) Rolled Curb, See STD Drawing BC-601-001
- (SE) Spot Elevations To Be Provided At Locations As Described Below:  
Beginning, Middle, And End Of Curves Approximately 40' Spacing  
Along Cul-De-Sac Edge Of Pavement Center Of Cul-De-Sac (High-Point)



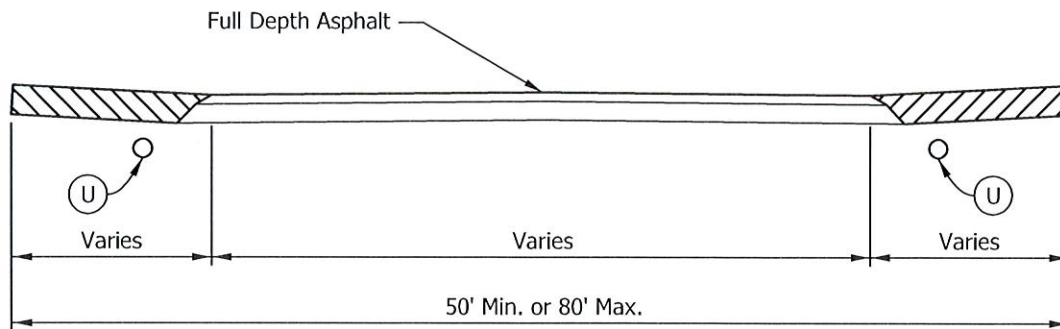
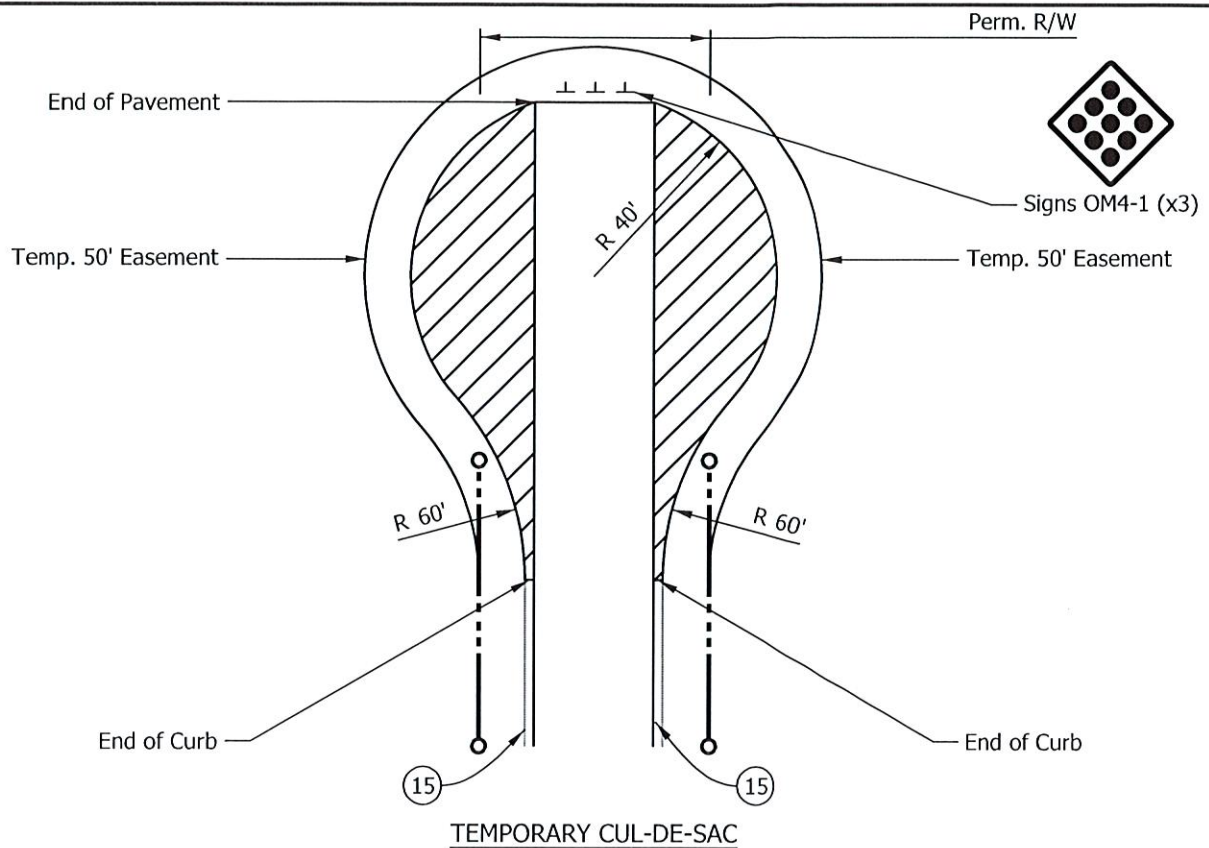
TYPE-2 (CUL-DE-SAC)

**BOONE COUNTY HIGHWAY DEPARTMENT**




**CUL-DE-SAC**

STANDARD DRAWING
NO. BC-604-001
SHEET 1 OF 1
SCALE: NO SCALE



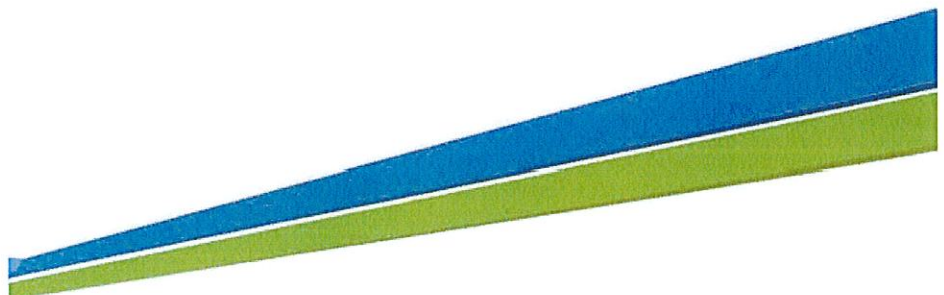
#### LEGEND

- (15) Concrete Curb And Gutter - See STD Drawing BC-601-001
- (U) Underdrain (Typ.) - See STD Drawing BC-401-001
-  Temporary Pavement To Be Removed When Street Is Continued. Thickness And Type To Match Permanent Pavement.

### BOONE COUNTY HIGHWAY DEPARTMENT

INDEX OF BOONE COUNTY STANDARDS  
700 SERIES STANDARD DRAWINGS  
STRUCTURES

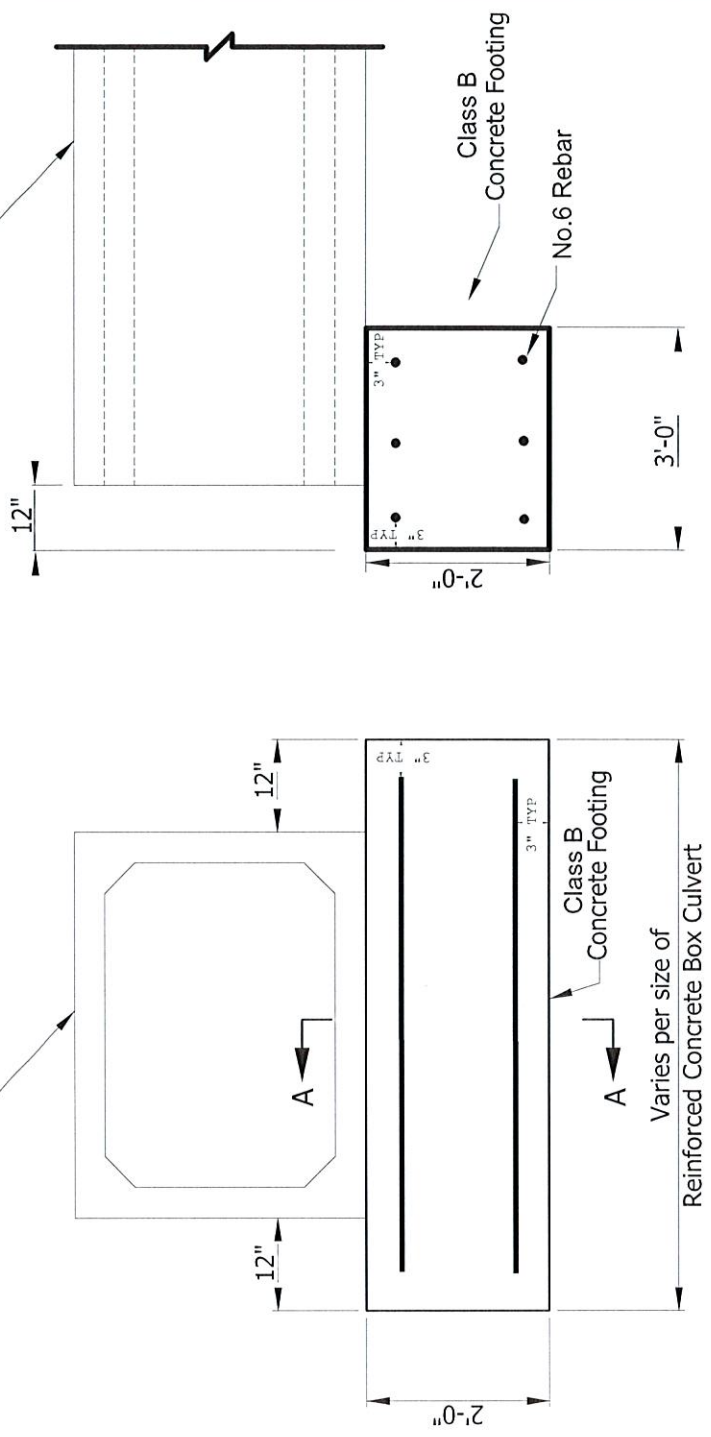
Reinforced Concrete Box Culvert Footing Details .....BC-714-001





Proposed Reinforced Concrete Box Culvert

Proposed Reinforced Concrete Box Culvert



SECTION "A-A"

PLAN VIEW

REVISED MAY 2024

BOONE COUNTY HIGHWAY DEPARTMENT



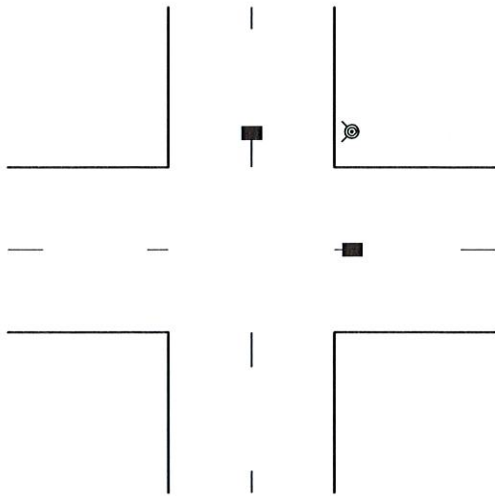
REINFORCED CONCRETE BOX CULVERT  
FOOTING DETAILS

STANDARD DRAWING
NO. BC-714-001
SHEET 1 OF 1
SCALE: NO SCALE

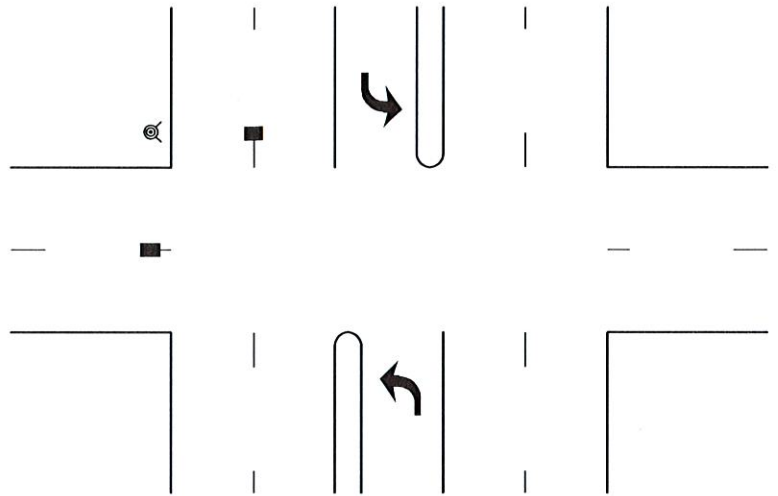
INDEX OF BOONE COUNTY STANDARDS  
800 SERIES STANDARD DRAWINGS  
TRAFFIC CONTROL DEVICES

Typical Blue RPM Locations.....	BC-800-001
Standard Barricade.....	BC-801-001
Typical Application For Work Beyond The Shoulder.....	BC-801-002
Typical Application For Shoulder Work With Minor Encroachment.....	BC-801-003
Typical Application For Lane Closure On A Two-Lane Road Using Flaggers.....	BC-801-004
Typical Application For Lane Closure On A Two-Lane Road With Low Traffic Volumes.....	BC-801-005
Typical Application For Temporary Road Closure.....	BC-801-006
Typical Application For Lane Closure On A Minor Street.....	BC-801-007
Typical Application Diagram For Additional Information.....	BC-801-008

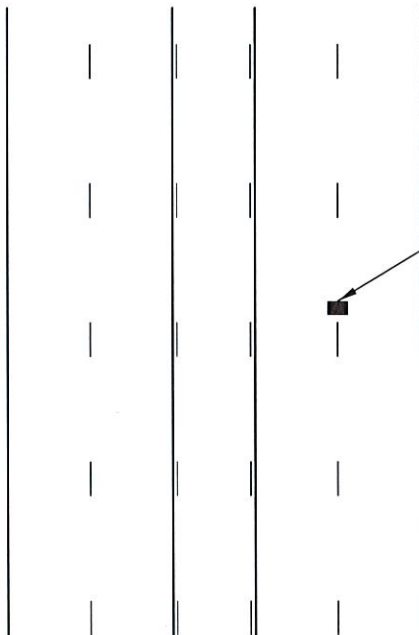




TWO-LANE STREET  
AT INTERSECTION



MULTI-LANE STREET  
AT INTERSECTION



MULTI-LANE STREET  
WITH TURN LANE OR  
MEDIAN

Blue Raised Pavement Marker Location (Typ.)

Fire Hydrant Location (Typ.)

## BOONE COUNTY HIGHWAY DEPARTMENT



RAISED PAVEMENT MARKER  
(BLUE)

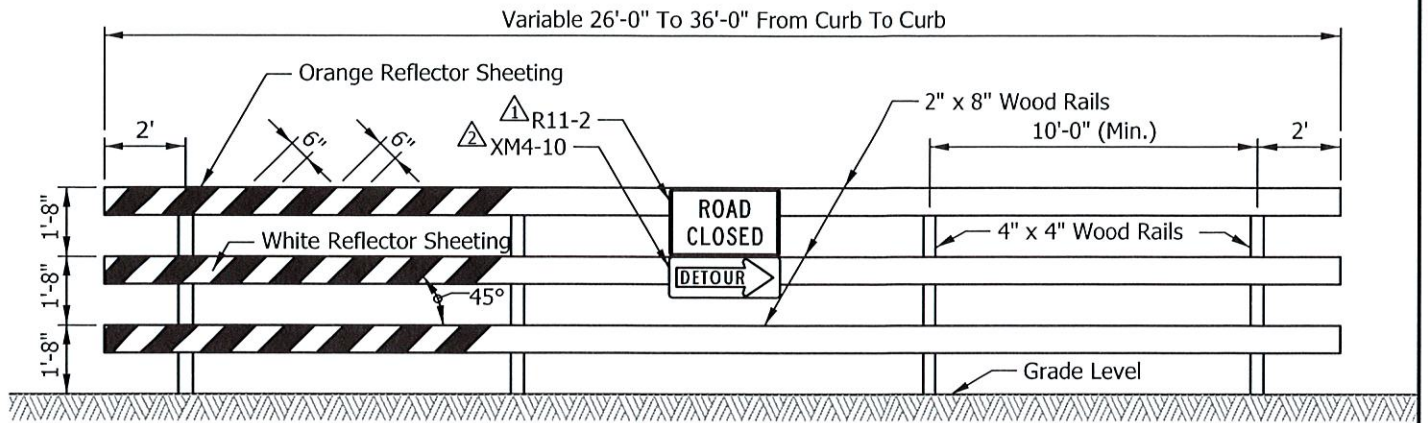
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NO. BC-800-001

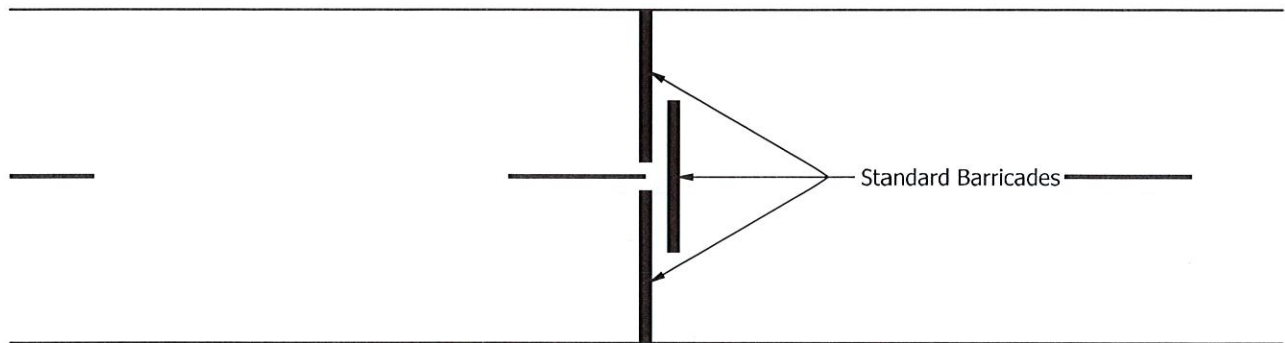
SHEET 1 OF 1

SCALE: NO SCALE





ROAD CLOSURE DETAIL  
ELEVATION VIEW



PLAN VIEW

NOTES:

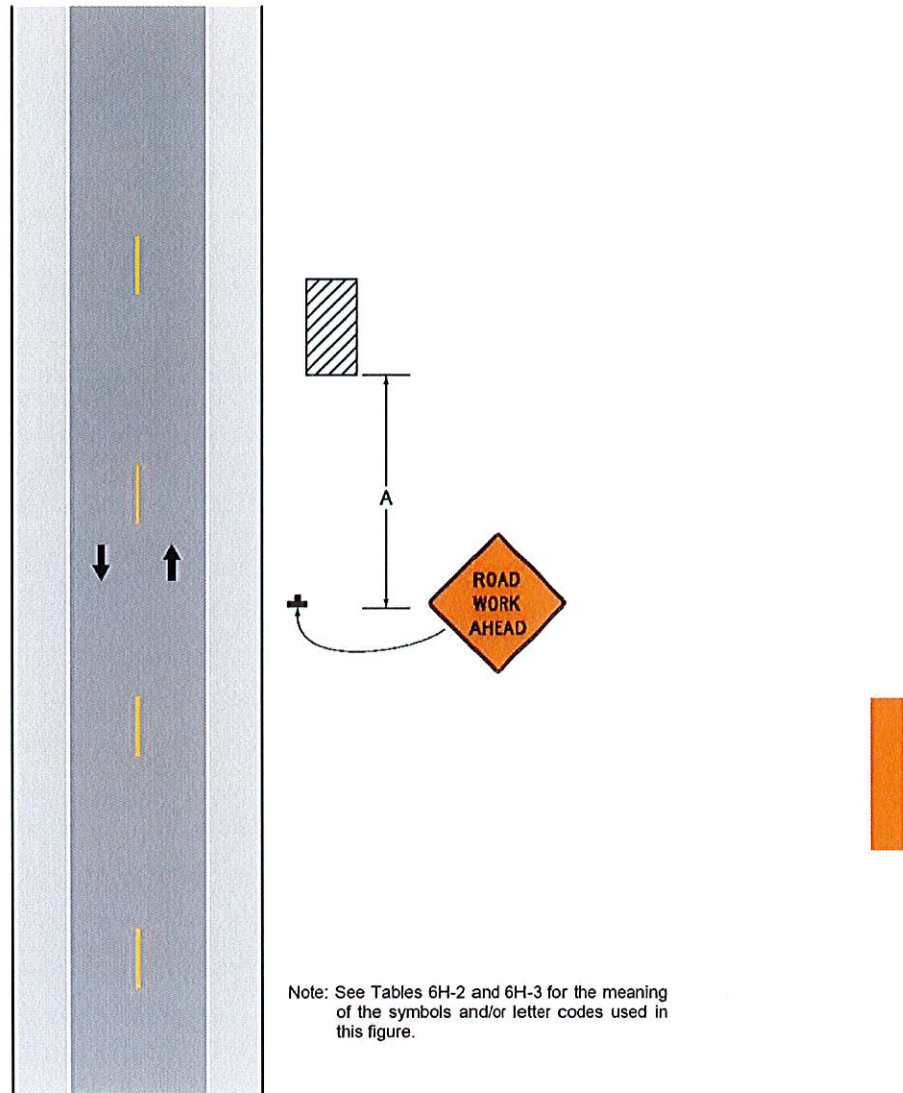
1. The Legend Of The R11-2 May Be Modified To "BRIDGE CLOSED" As Indicated On The Plans Or As Directed By The County Engineer.
2. The Detour Arrow Sign Shall Be Used Only When A Detour Route Has Been Signed.
3. Barricades, Lights, Signs, And Supports Shall Meet NCHRP 350 Or Mash Crash Evaluation Criteria.
4. All Wood Posts And Support Members Shall Be Painted With Two Coats Of White Paint.
5. Location Of Barricades Shall Be In Accordance With The Plans.
6. Reflective Sheeting Shall Be In Accordance With INDOT Standard Specifications.
7. Refer To Section 801 Of The INDOT Standard Specifications And The Latest Edition Of The Indiana Manual Of Uniform Traffic Control Devices (MUTCD) For Any Additional Information.

BOONE COUNTY HIGHWAY DEPARTMENT



STANDARD BARRICADE

STANDARD DRAWING  
NO. BC-801-001  
SHEET 1 OF 1  
SCALE: NO SCALE

**Figure 6H-1. Work Beyond the Shoulder (TA-1)****Typical Application 1**

November 2011

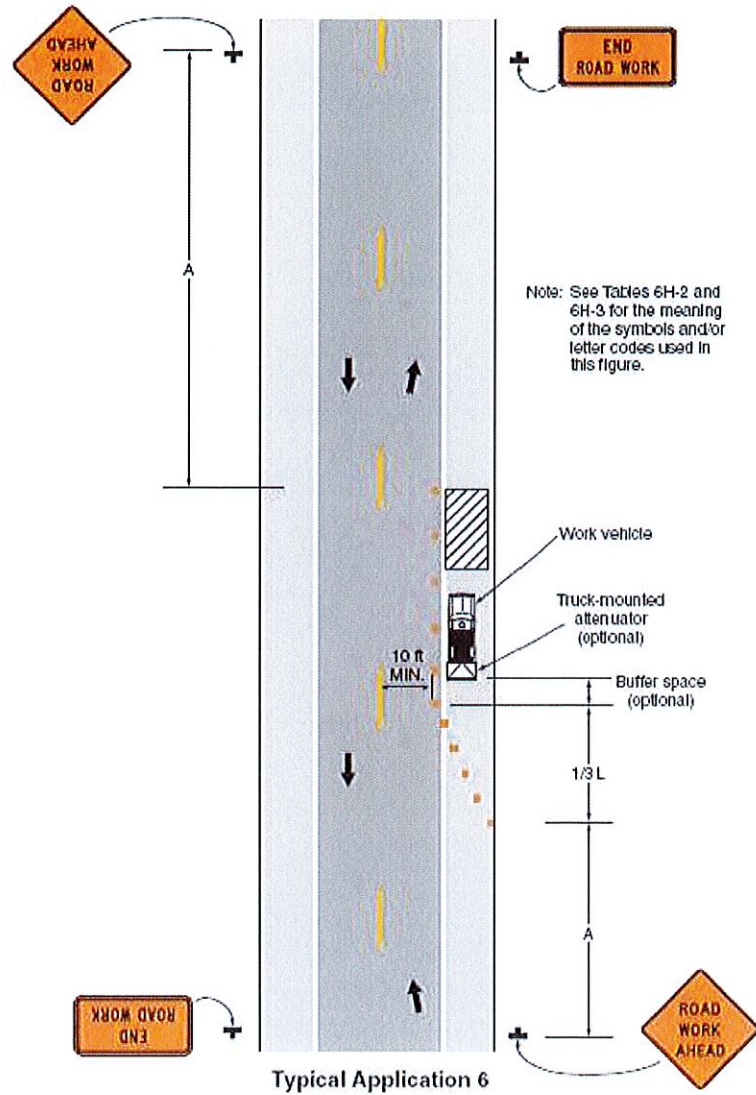
Sect. 6H.01

NOTES:

1. Figure From 2011 Indiana Manual On Uniform Traffic Control Devices (MUTCD)
2. For Additional Information See 2011 IMUTCD and See STD Drawing - BC-801-008

**BOONE COUNTY HIGHWAY DEPARTMENT**
**TYPICAL APPLICATION FOR  
WORK BEYOND THE SHOULDER**

STANDARD DRAWING
NO. BC-801-002
SHEET 1 OF 1
SCALE: NO SCALE

**Figure 6H-6. Shoulder Work with Minor Encroachment (TA-6)**

November 2011

Sect. 6H.01

**NOTES:**

- Figure From 2011 Indiana Manual On Uniform Traffic Control Devices (MUTCD)
- For Additional Information See 2011 IMUTCD and See STD Drawing - BC-801-008

**BOONE COUNTY HIGHWAY DEPARTMENT**
**TYPICAL APPLICATION FOR  
SHOULDER WORK WITH MINOR ENCROACHMENT**

STANDARD DRAWING

NO. BC-801-003

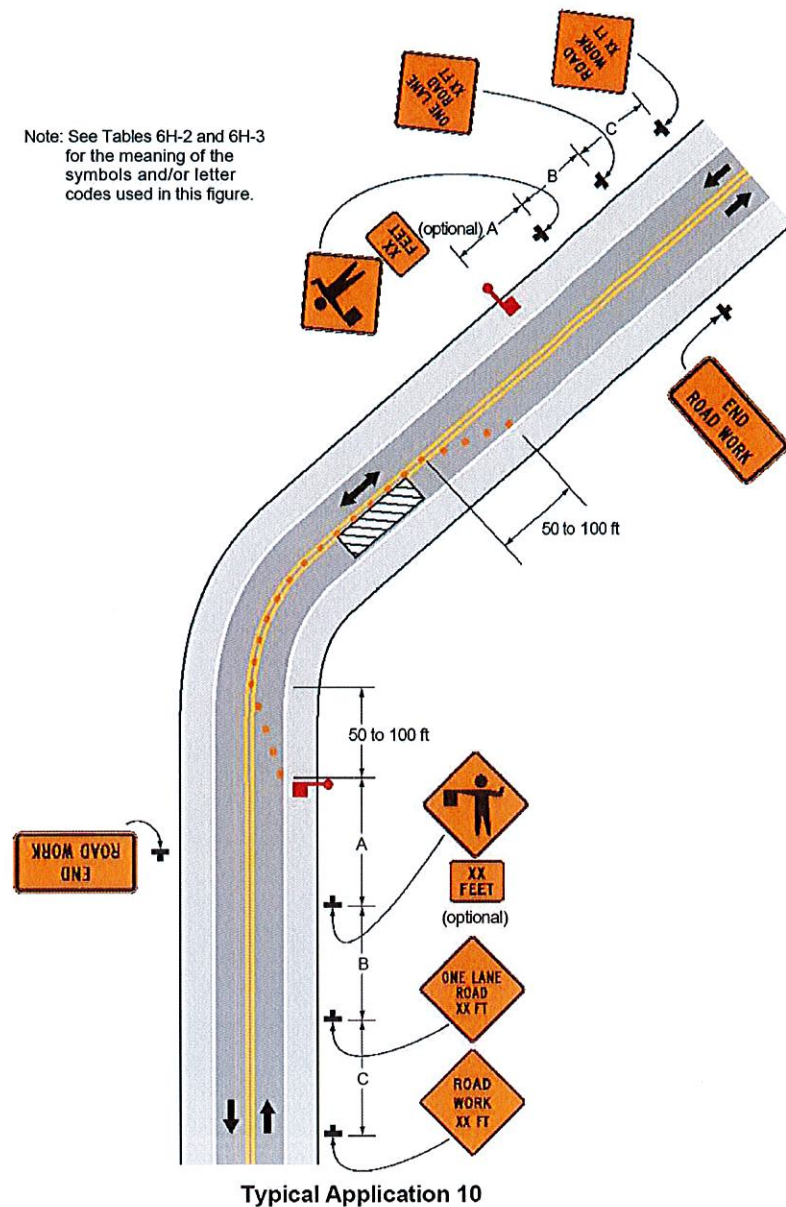
SHEET 1 OF 1

SCALE: NO SCALE



**Figure 6H-10. Lane Closure on a Two-Lane Road Using Flaggers (TA-10)**

Note: See Tables 6H-2 and 6H-3 for the meaning of the symbols and/or letter codes used in this figure.



November 2011

Sect. 6H.01

**NOTES:**

1. Figure From 2011 Indiana Manual On Uniform Traffic Control Devices (MUTCD)
2. For Additional Information See 2011 IMUTCD and See STD Drawing - BC-801-008

**BOONE COUNTY HIGHWAY DEPARTMENT**

**TYPICAL APPLICATION FOR  
LANE CLOSURE ON A TWO-LANE ROAD USING FLAGGERS**

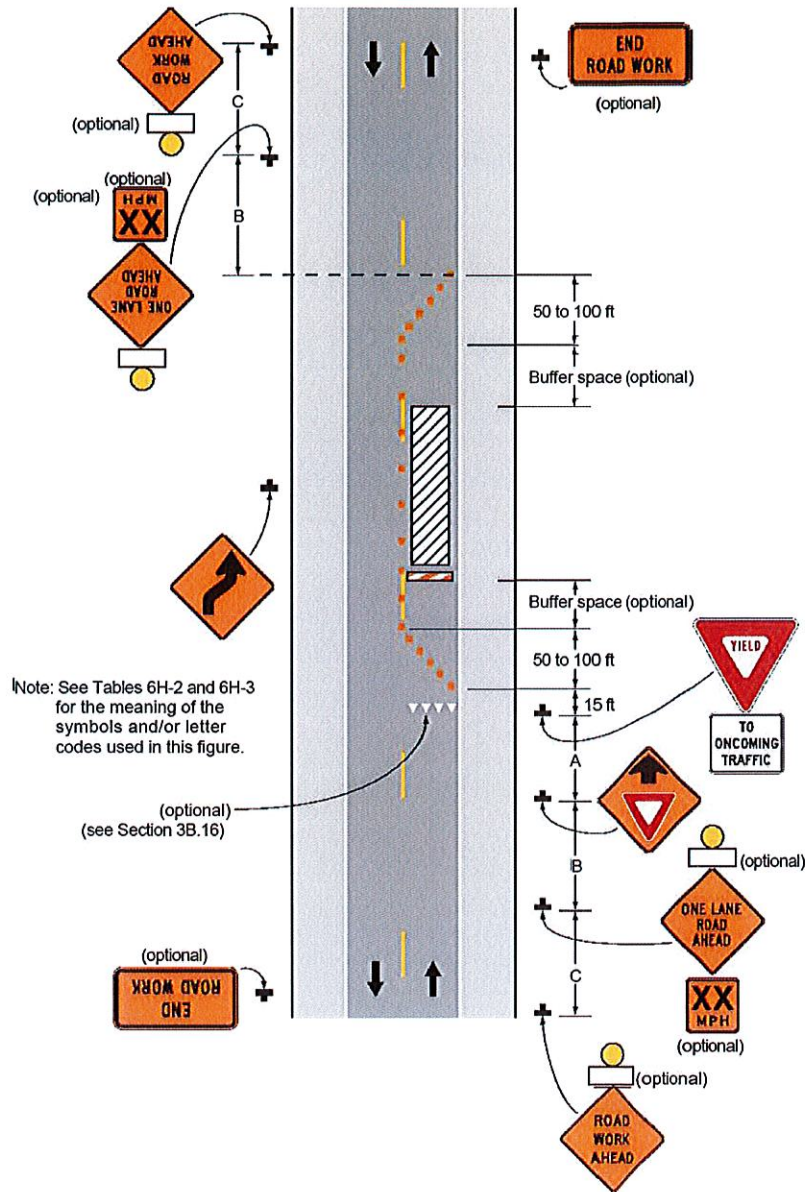
STANDARD DRAWING

NO. BC-801-004

SHEET 1 OF 1

SCALE: NO SCALE

**Figure 6H-11. Lane Closure on a Two-Lane Road with Low Traffic Volumes (TA-11)**



**Typical Application 11**

November 2011

Sect. 6H.01

**NOTES:**

- Figure From 2011 Indiana Manual On Uniform Traffic Control Devices (MUTCD)
- For Additional Information See 2011 IMUTCD and See STD Drawing - BC-801-008

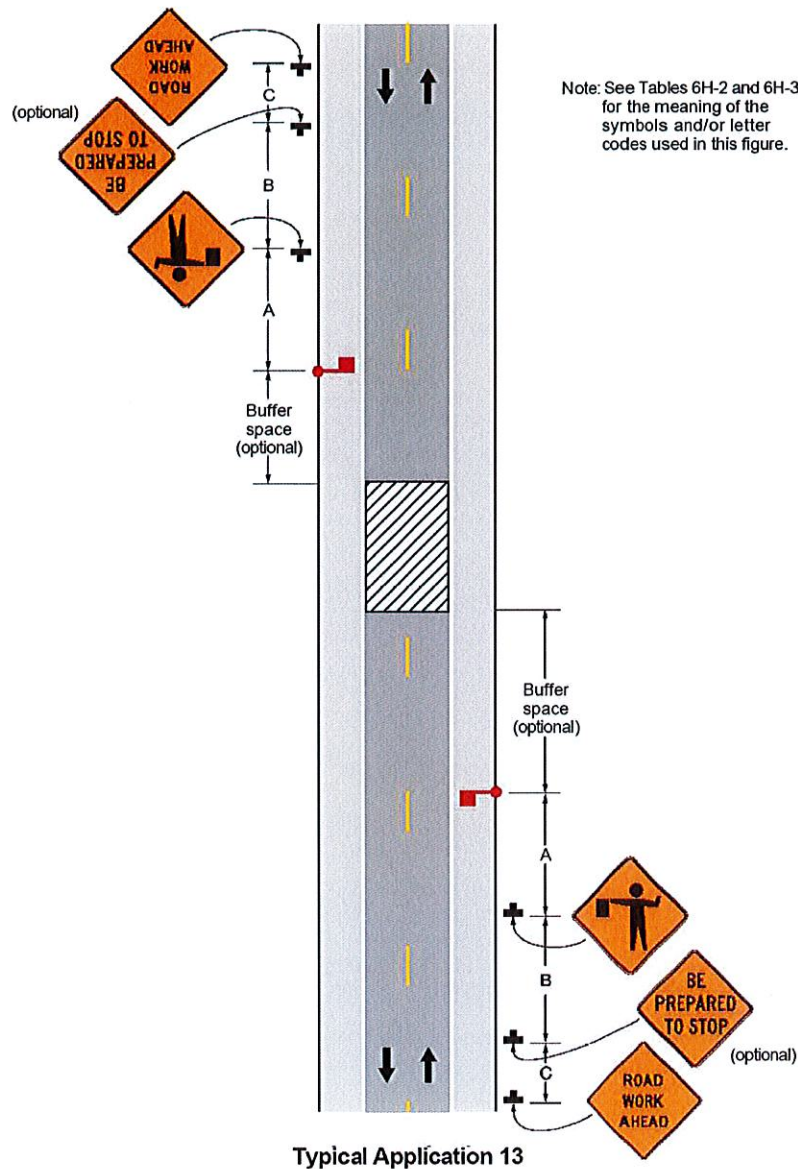
**BOONE COUNTY HIGHWAY DEPARTMENT**

**TYPICAL APPLICATION FOR  
LANE CLOSURE ON A TWO-LANE ROAD  
WITH LOW TRAFFIC VOLUME**



STANDARD DRAWING
NO. BC-801-005
SHEET 1 OF 1
SCALE: NO SCALE

Figure 6H-13. Temporary Road Closure (TA-13)



November 2011

Sect. 6H.01

NOTES:

1. Figure From 2011 Indiana Manual On Uniform Traffic Control Devices (MUTCD)
2. For Additional Information See 2011 IMUTCD and See STD Drawing - BC-801-008

## BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL APPLICATION FOR  
TEMPORARY ROAD CLOSURE

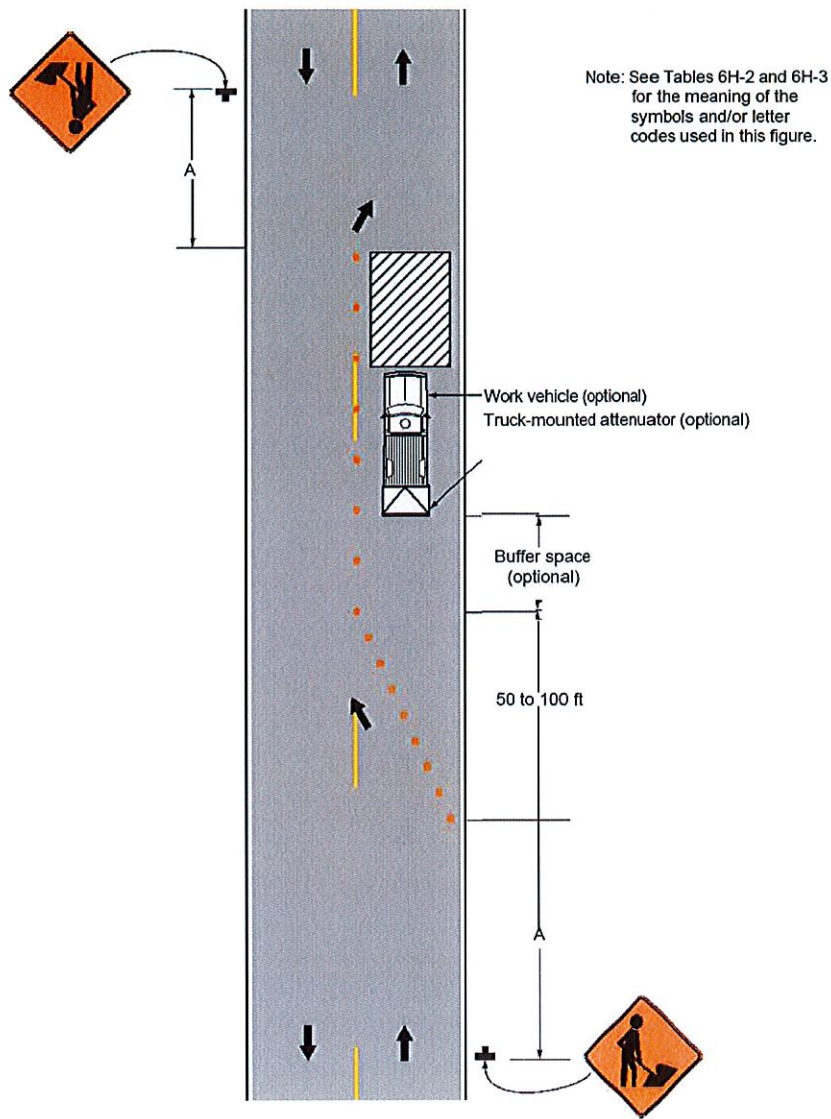
STANDARD DRAWING

NO. BC-801-006

SHEET 1 OF 1

SCALE: NO SCALE



**Figure 6H-18. Lane Closure on a Minor Street (TA-18)**

November 2011

Sect. 6H.01

**NOTES:**

1. Figure From 2011 Indiana Manual On Uniform Traffic Control Devices (MUTCD)
2. For Additional Information See 2011 IMUTCD and See STD Drawing - BC-801-008

**BOONE COUNTY HIGHWAY DEPARTMENT**

TYPICAL APPLICAITON FOR  
LANE CLOSURE ON A MINOR STREET


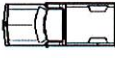





















STANDARD DRAWING

NO. BC-801-007

SHEET 1 OF 1

SCALE: NO SCALE

**Table 6H-2. Meaning of Symbols on Typical Application Diagrams**

	Arrow board		Shadow vehicle
	Arrow board support or trailer (shown facing down)		Sign (shown facing left)
	Changeable message sign or support trailer		Surveyor
	Channelizing device		Temporary barrier
	Crash cushion		Temporary barrier with warning light
	Direction of temporary traffic detour		Traffic or pedestrian signal
	Direction of traffic		Truck-mounted attenuator
	Flagger		Type 3 barricade
	High-level warning device (Flag tree)		Warning light
	Longitudinal channelizing device		Work space
	Luminaire		Work vehicle
	Pavement markings that should be removed for a long-term project		

**Table 6H-3. Meaning of Letter Codes on Typical Application Diagrams**

Road Type	Distance Between Signs**		
	A	B	C
Urban (low speed)*	100 feet	100 feet	100 feet
Urban (high speed)*	350 feet	350 feet	350 feet
Rural	500 feet	500 feet	500 feet
Expressway / Freeway	1,000 feet	1,500 feet	2,640 feet

\* Speed category to be determined by highway agency

\*\* The column headings A, B, and C are the dimensions shown in Figures 6H-1 through 6H-46. The A dimension is the distance from the transition or point of restriction to the first sign. The B dimension is the distance between the first and second signs. The C dimension is the distance between the second and third signs. (The "first sign" is the sign in a three-sign series that is closest to the TTC zone. The "third sign" is the sign that is furthest upstream from the TTC zone.)

**Table 6H-4. Formulas for Determining Taper Length**

Speed (S)	Taper Length (L) in feet
40 mph or less	$L = \frac{WS^2}{60}$
45 mph or more	$L = WS$

Where: L = taper length in feet  
W = width of offset in feet  
S = posted speed limit, or off-peak 85th-percentile speed prior to work starting, or the anticipated operating speed in mph

November 2011

Sect. 6H.01

**NOTES:**

- Figure From 2011 Indiana Manual On Uniform Traffic Control Devices (MUTCD)

**BOONE COUNTY HIGHWAY DEPARTMENT**
**ADDITIONAL INFORMATION FOR  
TYPICAL APPLICATION DIAGRAMS**

STANDARD DRAWING

NO. BC-801-008

SHEET 1 OF 1

SCALE: NO SCALE

INDEX OF BOONE COUNTY STANDARDS  
900 SERIES STANDARD DRAWINGS  
STREET SIGNS

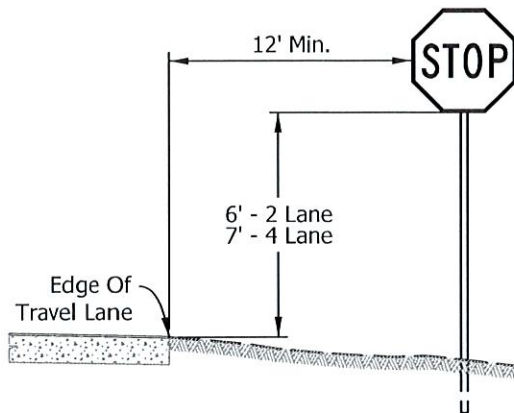
Typical Street Sign Locations.....BC-900-001

Typical Street Sign Details.....BC-900-002

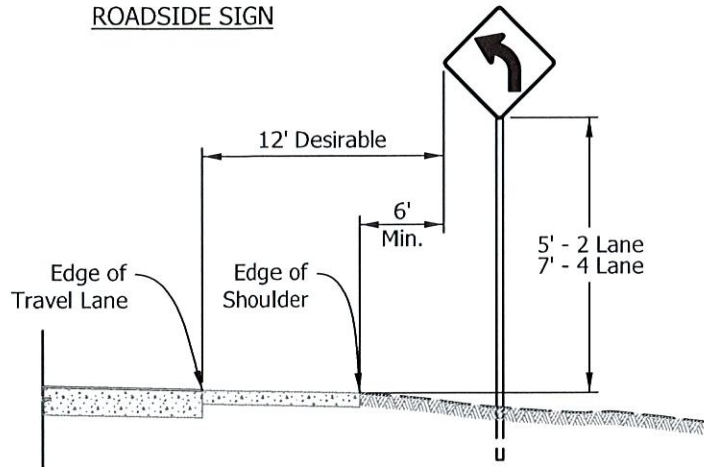




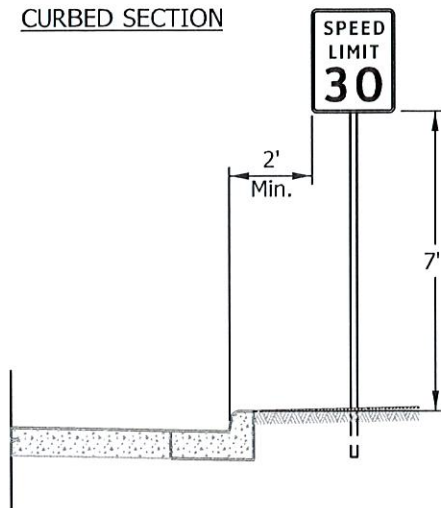
### STOP SIGN



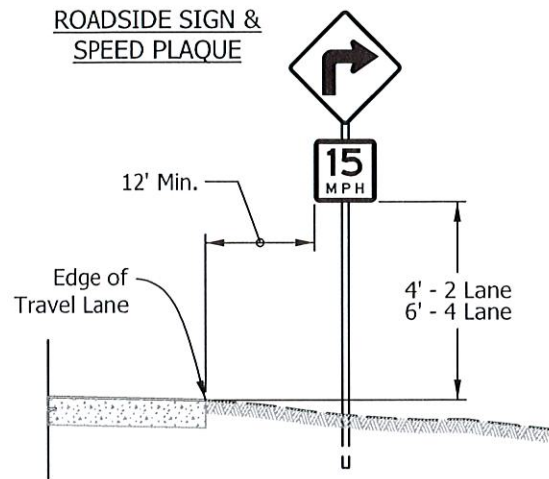
### ROADSIDE SIGN



### CURBED SECTION



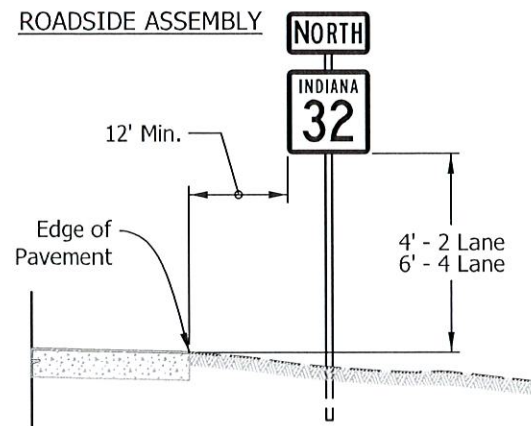
### ROADSIDE SIGN & SPEED PLAQUE



### NOTES:

1. See MUTCD Section 2A.19 For Reduced Lateral Offset Distances, 2' May Be Used In Areas Where Lateral Offsets Are Limited.
2. For Additional Information See MUTCD.
3. All Signs Shall Have H.I.P. Retroreflective Backgrounds.
4. All Sign Posts Shall Be Mounted On Breakaway Anchors.

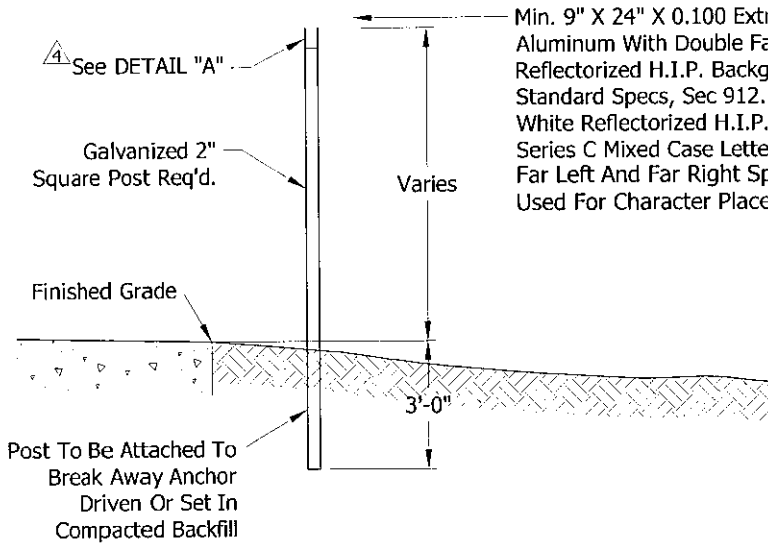
### ROADSIDE ASSEMBLY



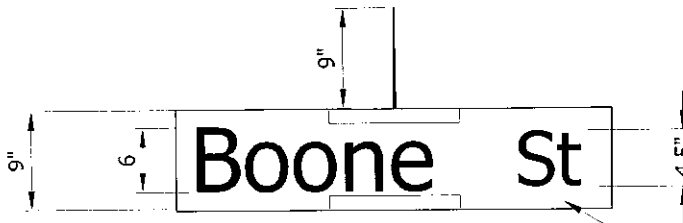
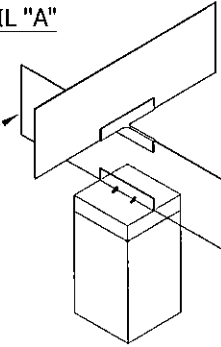
## BOONE COUNTY HIGHWAY DEPARTMENT

### POSITION OF SIGNS IN RELATION TO ROADWAY (CONVENTIONAL ROADS AND STREETS)

## STREET SIGN



## DETAIL "A"



## STREET SIGN SIZE

Min. 9" X 24" X 0.100 Extruded Aluminum With Double Faced Green ReflectORIZED H.I.P. Background Per Standard Specs, Sec 912.10 And 6" White ReflectORIZED H.I.P. Type IV, Series C Mixed Case Letters. Far Left And Far Right Spacing Shall Be Used For Character Placement.

1. Private Streets Shall Be So Indicated On The Left Hand Side Of The Sign With Dual Line 1-1/2" Black Letters On Yellow ReflectORIZED Background Preceding The Street Name.
2. One Sign Assemble Shall Be Located At Each Intersection On The Northeast Corner When Possible.
3. Stop Signs Shall Not Be Installed On The Street Name Sign Posts.
4. All Hardware To Be Galvanized Steel
5. For Additional Information See MUTCD.
6. All Sign Posts Shall Be Mounted On Breakaway Anchors.

REVISED MAY 2024

## BOONE COUNTY HIGHWAY DEPARTMENT



## STREET NAME SIGNS (DETAILS)

STANDARD DRAWING  
NO. BC-900-002  
SHEET 1 OF 1  
SCALE: NO SCALE