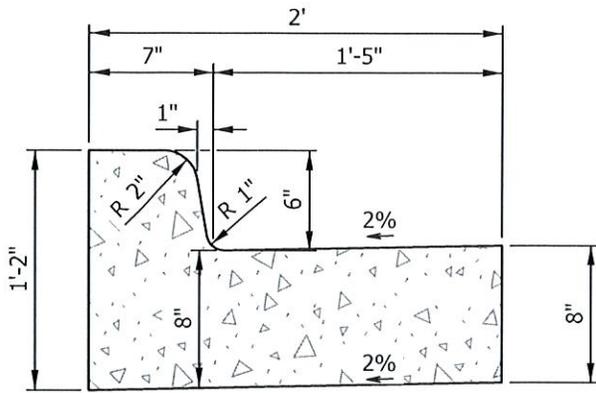


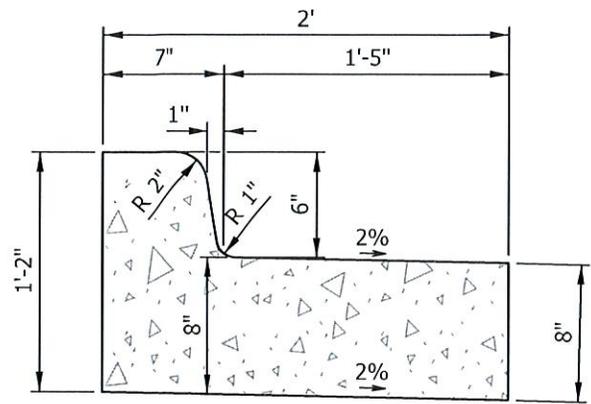
INDEX OF BOONE COUNTY STANDARDS  
600 SERIES STANDARD DRAWINGS  
INCIDENTAL CONSTRUCTION

Typical Concrete Curb and Gutter and Concrete Curb.....	BC-601-001
Typical Concrete Sidewalk Detail.....	BC-602-001
Typical HMA Multi-Use Path.....	BC-602-002
Typical Subdivision Residential Drive.....	BC-603-001
Typical Urban Commercial Drive.....	BC-603-002
Typical Field Entrance.....	BC-603-003
Typical Rural Residential Drive.....	BC-603-004
Typical Rural Commercial Drive.....	BC-603-005
Typical Subdivision Entrance.....	BC-603-006
Typical Street Approach.....	BC-603-007
Typical Acceleration/Deceleration Lane.....	BC-603-008
Typical Mailbox Approach.....	BC-603-009
Typical Culvert Detail With Open Ditches.....	BC-603-010
Typical Cul-De-Sac.....	BC-604-001
Typical Temporary Cul-De-Sac.....	BC-604-002

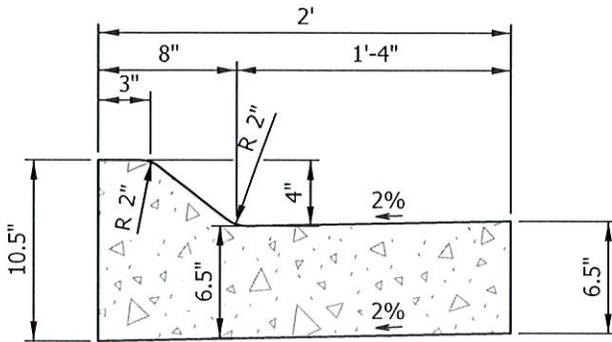
3/27/2019 7:24:02 AM I:\P\170004 - Boone County As-Needed Assistance\Standards Consolidation\09Road\04 Design files\02 DGN\Plans\600s\601-001\_BC\_Concrete Curb and Gutter\_Sht\_Typical.dgn



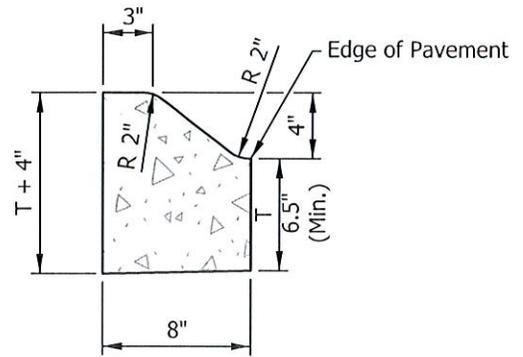
**CONCRETE CURB AND GUTTER  
STANDARD (Typ.)**



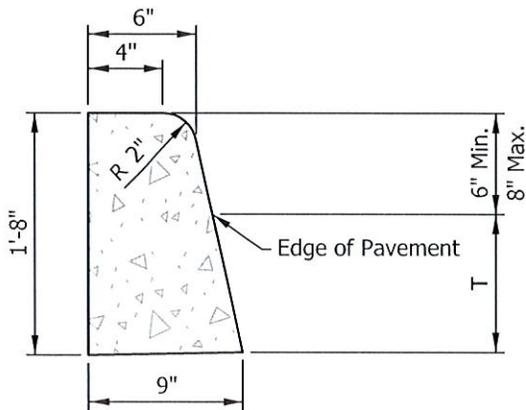
**CONCRETE CURB AND GUTTER  
MODIFIED (Typ.)**



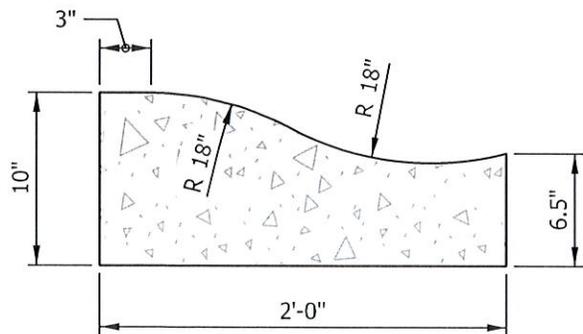
**CONCRETE CURB AND GUTTER  
MOUNTABLE (Typ.)**



**CONCRETE CURB  
MOUNTABLE**



**CONCRETE CURB  
BARRIER**



**ROLLED CURB**

**NOTES**

T = Nominal Pavement Thickness

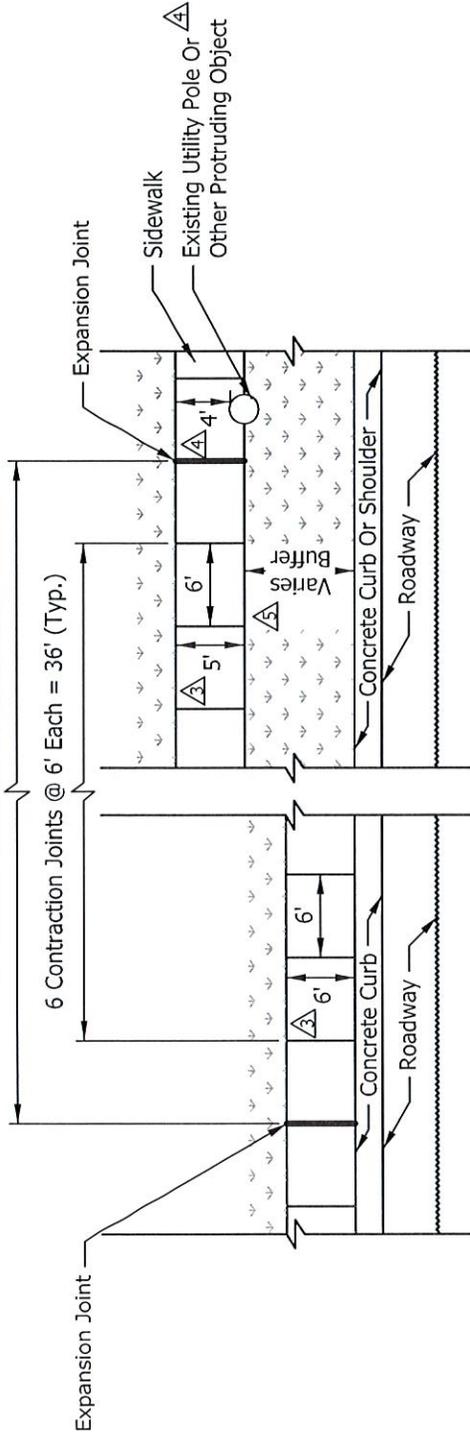
**BOONE COUNTY HIGHWAY DEPARTMENT**

**CONCRETE CURB AND GUTTER  
AND CURB**



STANDARD DRAWING
NO. BC-601-001
SHEET 1 OF 1
SCALE: NO SCALE

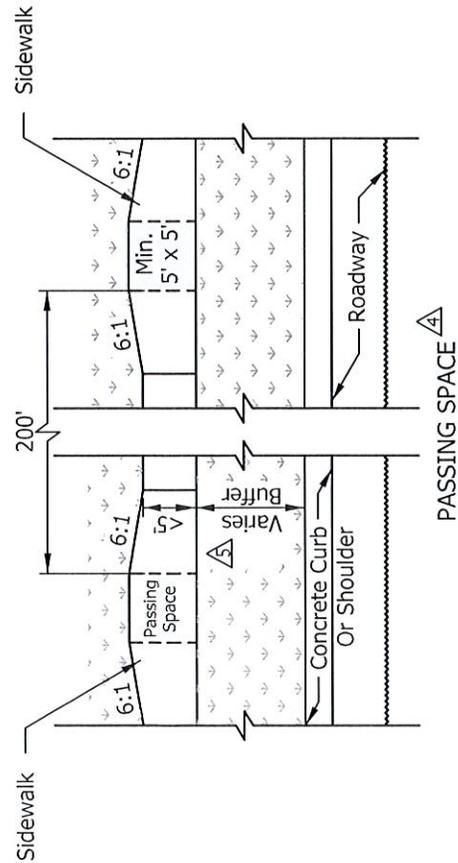
48' Between Sidewalk Expansion Joints



SIDEWALK PLAN VIEW

NOTES:

1. All Slopes Are Absolute Rather Than Relative To The Sidewalk Or Roadway Grade And Are Given As Maximums. Slopes At Least 0.50% Less Than The Maximum Are Preferred.
2. The Grade Of The Sidewalk Is Measured In The Direction Of Pedestrian Travel And Shall Not Exceed The Grade Of The Adjacent Roadway. The Cross Slope Is Measured Perpendicular To The Direction Of Pedestrian Travel And Shall Not Exceed 2.00%
  - ⚠ Where There Is A Buffer Between The Sidewalk And The Curb, The Preferred Minimum Sidewalk Clear Width Is 5'. Where There Is No Buffer Between The Sidewalk And The Curb, The Preferred Minimum Sidewalk Width Is 6'.
  - ⚠ A 4' Minimum Clear Width Shall Be Provided Adjacent To Street Furniture, Mailbox, Utility Pole, Or Other Protruding Object. Where The Sidewalk Clear Width Is Less Than 5', A Passing Space With Dimensions Of 5' X 5' Shall Be Provided At 200' Intervals
  - ⚠ See Roadway Typical Sections for Dimensions



LEGEND:

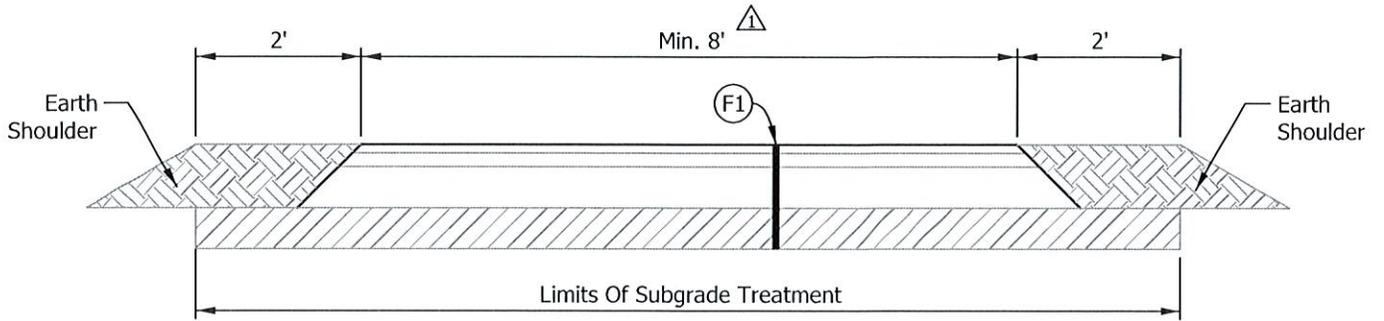
- (F) 4" Sidewalk, Concrete On 4" Compacted Aggregate No. 53

BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL CONCRETE SIDEWALK



STANDARD DRAWING
NO. BC-602-001
SHEET 1 OF 1
SCALE: NO SCALE



LEGEND:

- (F1) 1.5" HMA Surface, 9.5mm On  
 2.5" HMA Intermediate, 19.0mm On  
 6.0" Compacted Aggregate, No. 53 On  
 Typ. INDOT Subgrade Treatment Type III

NOTES:

- △ Width Varies, Typ. 8' - 12'  
 Cross Slope Varies, Typ. Max. 2.0%

BOONE COUNTY HIGHWAY DEPARTMENT



TYPICAL HMA MULTI-USE PATH

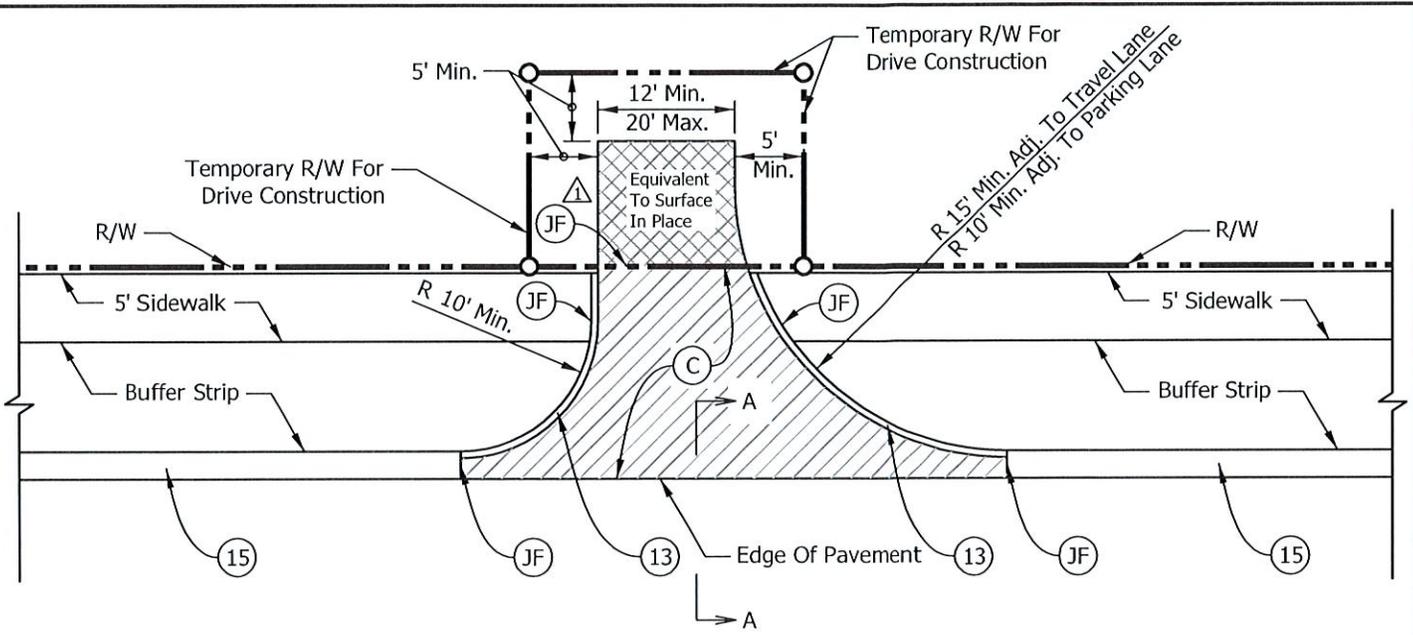
STANDARD DRAWING

NO. BC-602-002

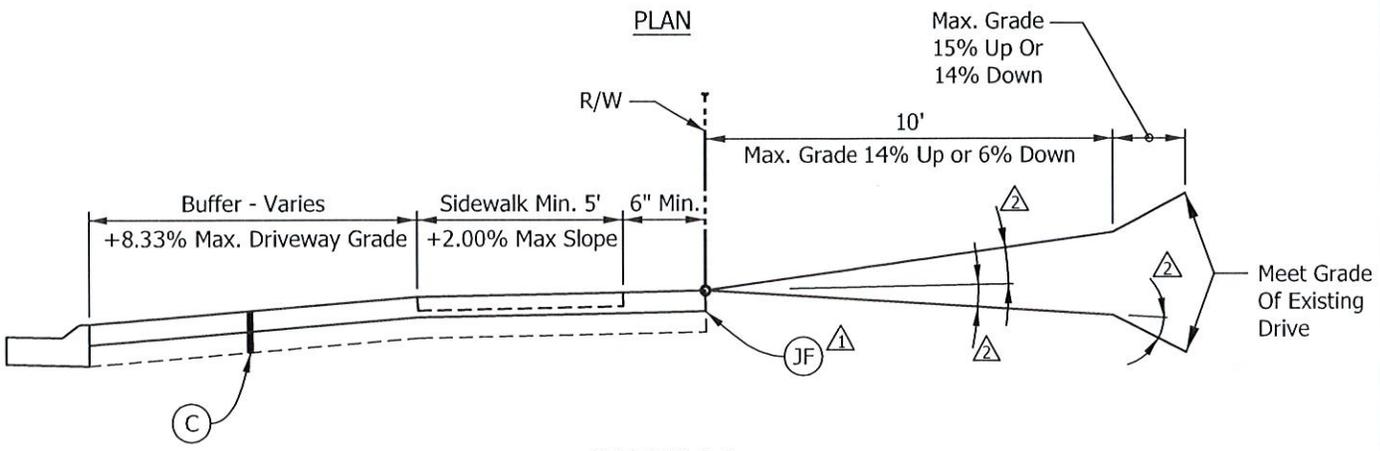
SHEET 1 OF 1

SCALE: NO SCALE

3/27/2019 7:24:03 AM I:\170004 - Boone County As-Needed Assistance\Standards Consolidation\09Road\04 Design Files\02 DGN\Plans\603-001\_BC\_Res\_Drive\_Sht\_Typical.dgn



PLAN



SECTION A-A

LEGEND:

(C)	Pavement Layer	Asphalt	Concrete	Aggregate
	HMA Surface, Type B	-	-	-
	HMA Intermediate, Type B	-	-	-
	HMA Base, Type B	-	-	-
	Compacted Aggregate No. 53	-	-	-
	INDOT Subgrade Treatment	-	Type II	8" (3)
	PCCP for Approaches	N/A	6"	N/A

(JF) 1/2" Preformed Joint Filler

(13) Monolithic Curb See STD Drawing BC-601-001

(15) Concrete Curb And Gutter See STD Drawing BC-601-001

NOTES:

- (1) Joint Filler Required When Existing Drive Is PCCP
- (2) Max. Algebraic Diff. In Grades;  
Crests 8.00%  
Sags 12.00%
- (3) 8" Compacted Aggregate May Be Substituted For PCCP With Approval From County Engineer
- 4. Sidewalk Elevation Transitions At Drives To Be In Compliance With Current ADA Guidelines
- 5. Driveway Culverts Which Cannot Be located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards
- 6. For Drainage Structure Details See STD Drawing BC-603-010

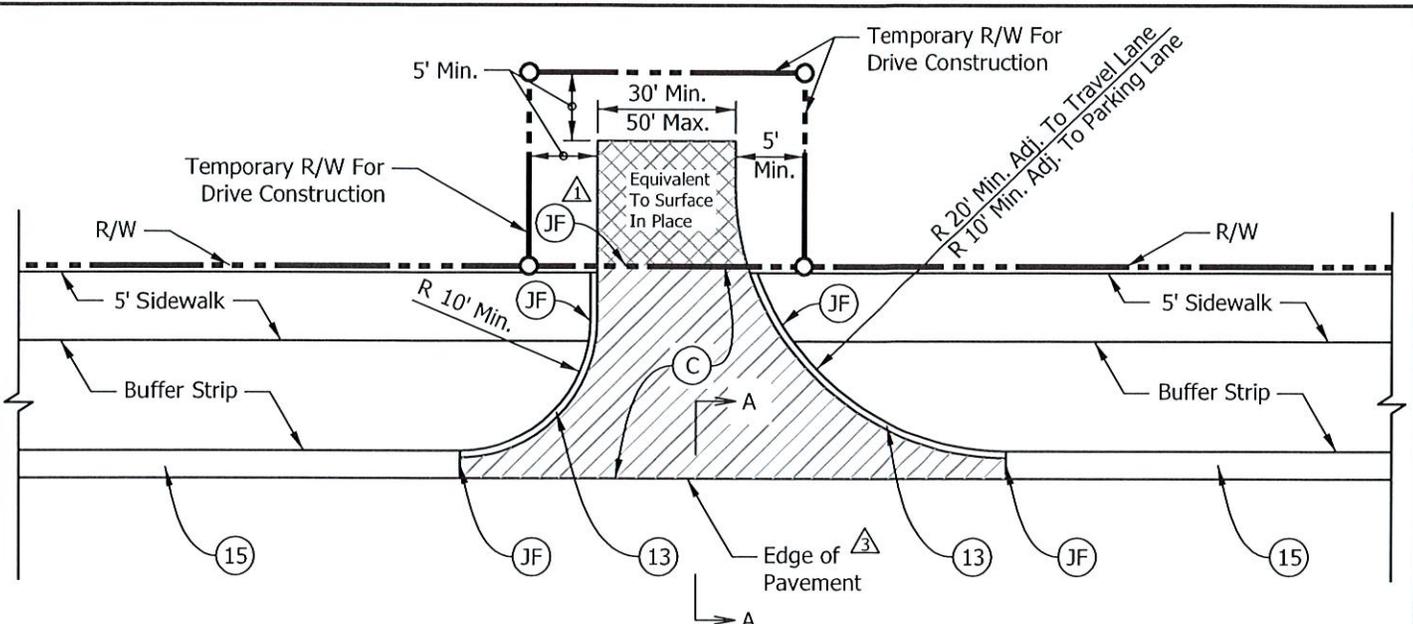
BOONE COUNTY HIGHWAY DEPARTMENT



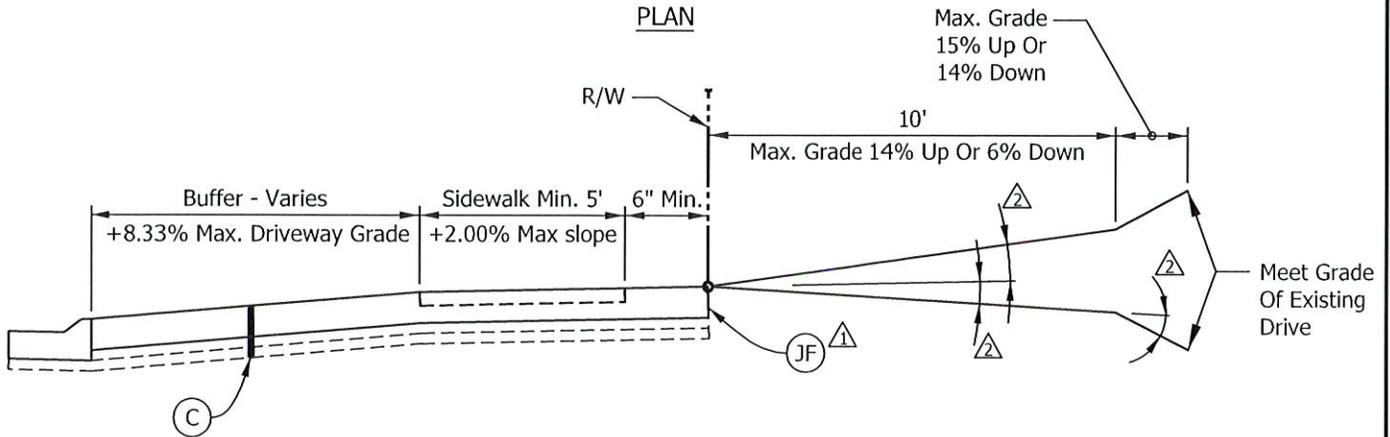
TYPICAL SUBDIVISION RESIDENTIAL DRIVEWAY

STANDARD DRAWING
NO. BC-603-001
SHEET 1 OF 1
SCALE: NO SCALE

3/27/2019 7:24:04 AM I:\170004 - Boone County As-Needed Assistance\Standards Consolidation\09Road\04 Design files\02 DCN\Plans\600s\603-002\_BC\_Commercial Drive\_Sht\_Typical.dgn



PLAN



SECTION A-A

LEGEND:

(C)	Pavement Layer	Asphalt	Concrete	Aggregate
	HMA Surface, Type B	-	-	-
	HMA Intermediate, Type B	-	-	-
	HMA Base, Type B	-	-	-
	Compacted Aggregate No. 53	-	-	-
	INDOT Subgrade Treatment	-	Type II	-
	PCCP for Approaches	N/A	9"	N/A

- (JF) 1/2" Preformed Joint Filler
- (13) Monolithic Curb See STD Drawing BC-601-001
- (15) Concrete Curb And Gutter See STD Drawing BC-601-001

NOTES:

- △ Joint Filler Required When Existing Drive Is PCCP
- △ Max. Algebraic Diff. In Grades; Crests 8.00% Sags 12.00%
- △ If Warranted By County Engineer Acceleration/Deceleration Taper To Be Added, See STD Drawing BC-603-008
- 4. Sidewalk Elevation Transitions At Drives To Be In Compliance With Current ADA Guidelines
- 5. Driveway Culverts Which Cannot Be Located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards
- 6. Commercial Drives Shall Be Required At Truck Stops And Entrances To Heavy Industrial Properties
- 7. For Drainage Structure Details See STD Drawing BC-603-010

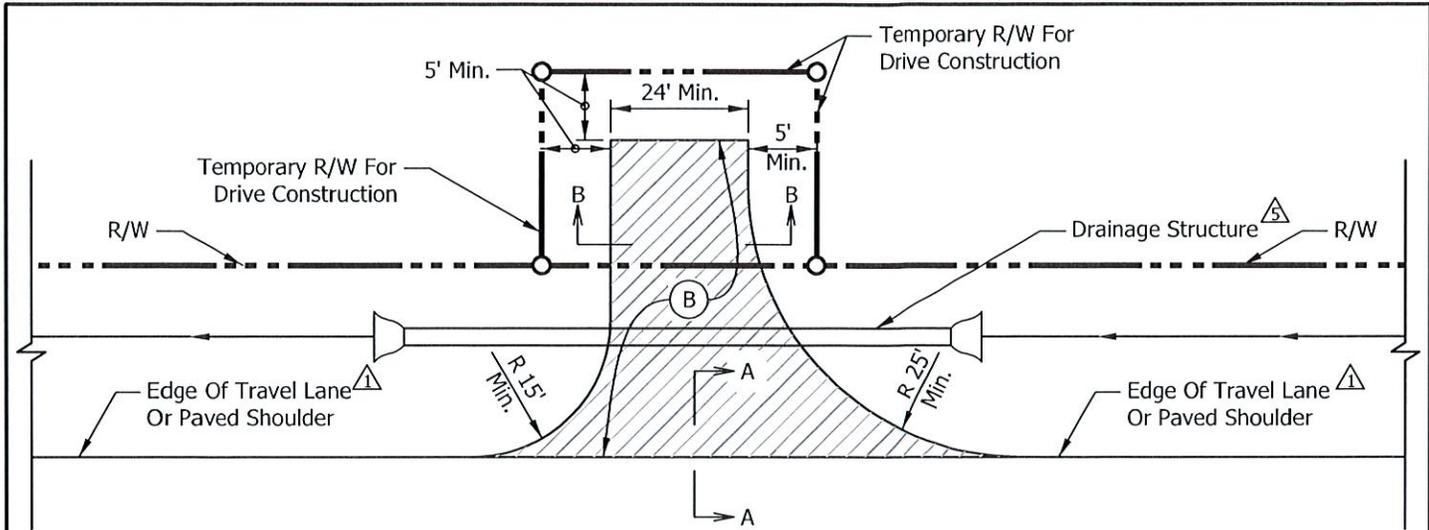
BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL URBAN COMMERCIAL DRIVEWAY

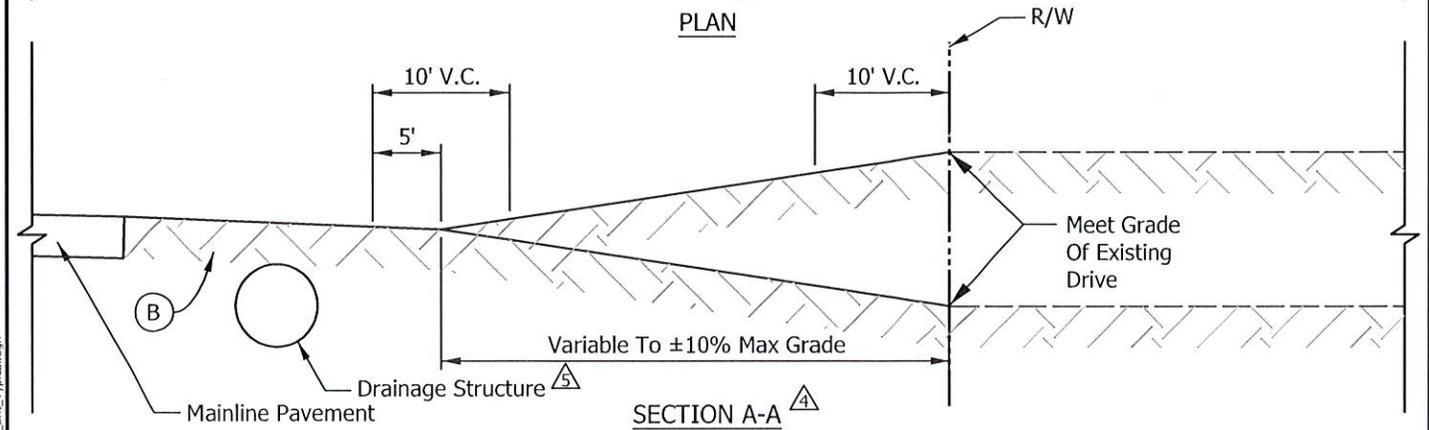


STANDARD DRAWING
NO. BC-603-002
SHEET 1 OF 1
SCALE: NO SCALE

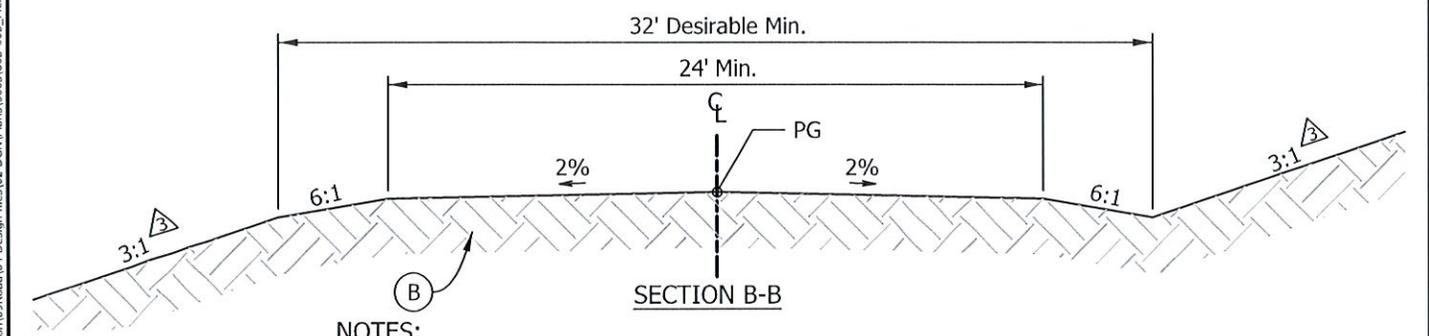
3/27/2019 7:24:04 AM I:\170004 - Boone County As-Needed Assistance\Standards\09Road\04 Design files\02.DCN\Plans\6003\603-003\_Field Drive\_Sht\_Typical.dgn



PLAN



SECTION A-A



SECTION B-B

**NOTES:**

- 1. Where The Shoulder Is Earth Or Aggregate, Or When The Paved Shoulder Is Less Than 8', The Drive Radii Shall Be Tangent To The Travel Lane. Where The Paved Shoulder Is 8' Or More, The Drive Radii Shall Be Tangent To The Paved Shoulder
- 2. Driveway Culverts Which Cannot Be Located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards
- 3. Foreslopes And Backslopes Shall Be In Accordance With Clear Zone Requirements Set Forth By INDOT Design Manual (Current Edition)
- 4. Minimum 2% Grade For 10' Away From Edge Of Pavement Required For Gravel Drives
- 5. For Drainage Structure Details - See STD Drawing BC-603-010

**LEGEND:**

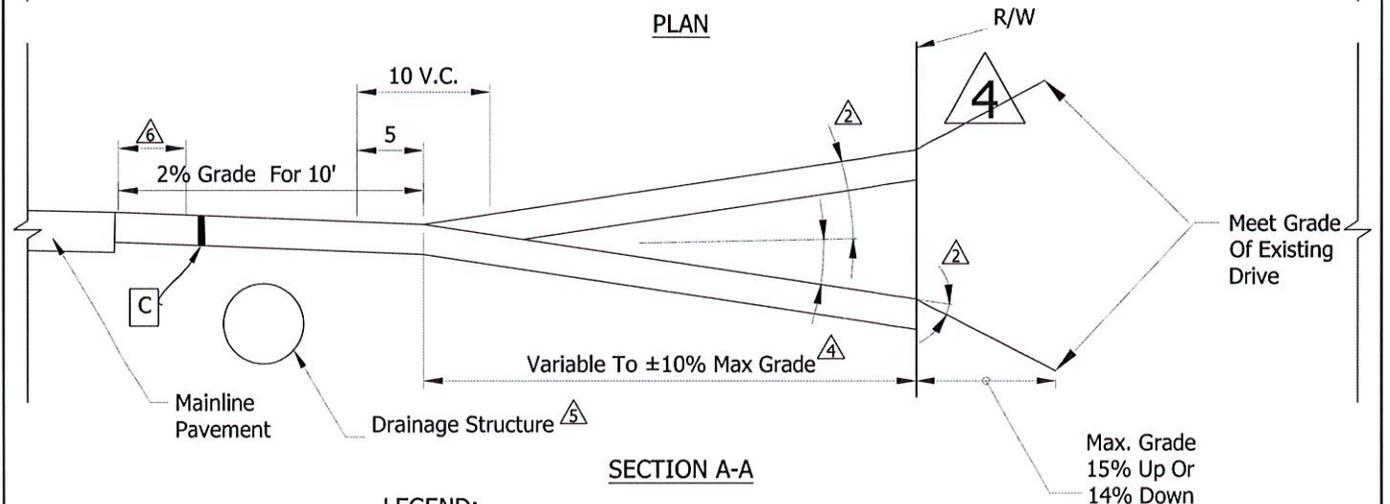
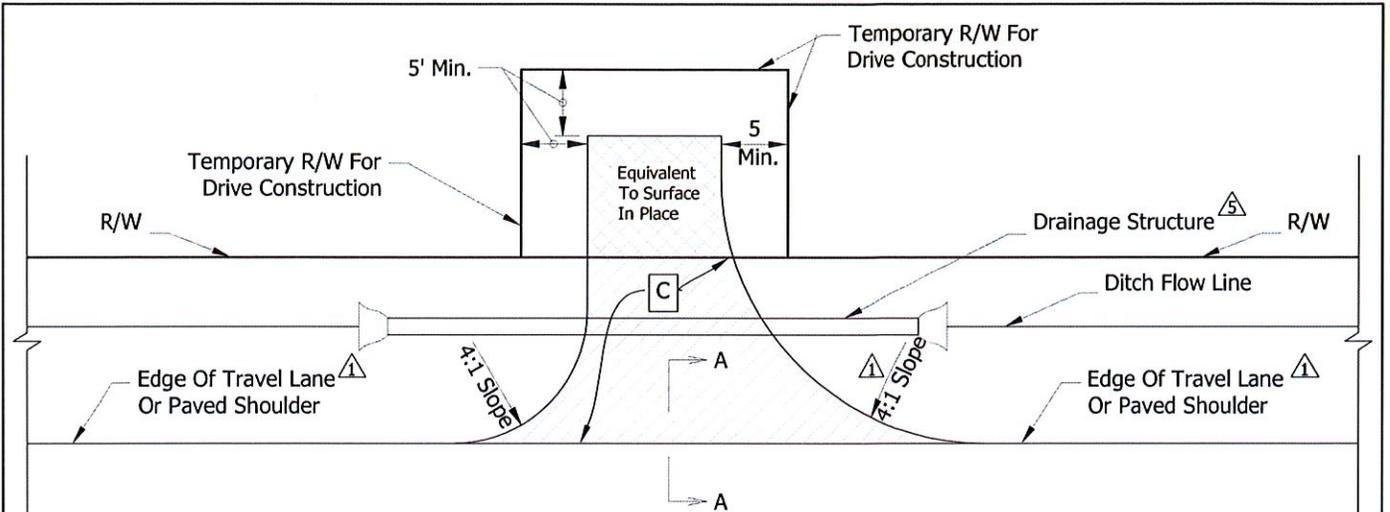
(B) Earth Fill Or Aggregate As Needed

**BOONE COUNTY HIGHWAY DEPARTMENT**



**TYPICAL FIELD ENTRANCE**

STANDARD DRAWING
NO. BC-603-003
SHEET 1 OF 1
SCALE: NO SCALE



**LEGEND:**

Pavement Layer	Asphalt	Concrete	Aggregate	Mainline Resurfacing
HMA Surface, Type B	1.5"	-	-	per plan
HMA Intermediate, Type B	3.5"	-	-	per plan
HMA Base, Type B	-	-	-	N/A
Compacted Aggregate No. 53	-	-	6"	N/A
INDOT Subgrade Treatment	Type II	Type II	Type II	N/A
PCCP for Approaches	N/A	6"	N/A	N/A
Minimum Width for HMA Approach	N/A	2'	N/A	2'

**NOTES:**

- 1. Where The Shoulder Is Earth Or Aggregate, Or When The Paved Shoulder Is Less Than 8', The Drive Radii Shall Be Tangent To The Travel Lane. Where The Paved Shoulder Is 8' Or More, The Drive Radii Shall Be Tangent To The Paved Shoulder
- 2. Max. Algebraic Diff. In Grades; Crests 8.00%, Sags 12.00%
- 3. Driveway Culverts Which Cannot Be located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards. Drainage Structure Should Be Minimum Of 10' From Edge Of Pavement (Clear Zone)
- 4. Minimum 2% Grade For 10' Away From Edge Of Pavement Required For All Drives. 2.5" For Every 10' From Edge Of Pavement
- 5. For Drainage Structure Details - See STD Drawing BC-603-010

**BOONE COUNTY HIGHWAY DEPARTMENT**

**TYPICAL RURAL RESIDENTIAL DRIVEWAY**

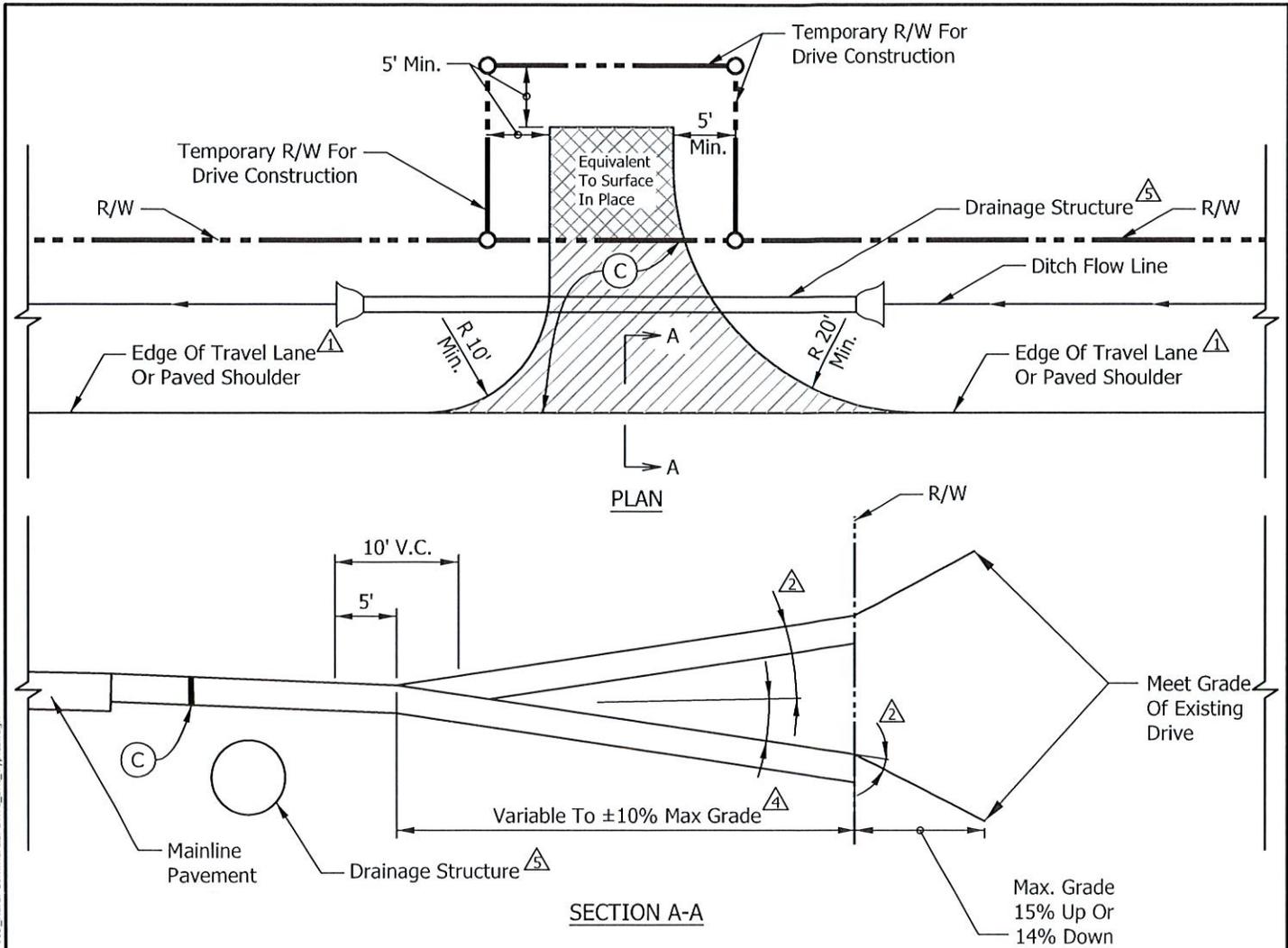
STANDARD DRAWING  
NO. BC-603-004  
SHEET 1 OF 1  
SCALE: NO SCALE



REVISED MAY 2021

SOURCES: STIMES, \$FILELS

3/27/2019 7:24:05 AM I:\170004 - Boone County As-Needed Assistance\Standards Consolidation\09Road\04 Design files\02 DGN\Plans\603-005\_Rural Commercial Drive\_Sht\_Typical.dgn



**LEGEND:**

C	Pavement Layer	Asphalt	Concrete	Aggregate
	HMA Surface, Type B	1.5"	-	-
HMA Intermediate, Type B	2.5"	-	-	
HMA Base, Type B	8.0"	-	-	
Compacted Aggregate No. 53	-	-	12"	
INDOT Subgrade Treatment	Type II	Type II	Type II	
PCCP for Approaches	N/A	9"	N/A	

**NOTES:**

- △ Where The Shoulder Is Earth Or Aggregate, Or When The Paved Shoulder Is Less Than 8', The Drive Radii Shall Be Tangent To The Travel Lane. Where The Paved Shoulder Is 8' Or More, The Drive Radii Shall Be Tangent To The Paved Shoulder
- △ Max. Algebraic Diff. In Grades; Crests 8.00%, Sags 12.00%
- 3. Driveway Culverts Which Cannot Be located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards
- △ Minimum 2% Grade For 10' Away From Edge Of Pavement Required For Gravel Drives
- △ For Drainage Structure Details - See STD Drawing BC-603-010

**BOONE COUNTY HIGHWAY DEPARTMENT**



**TYPICAL RURAL COMMERCIAL DRIVEWAY**

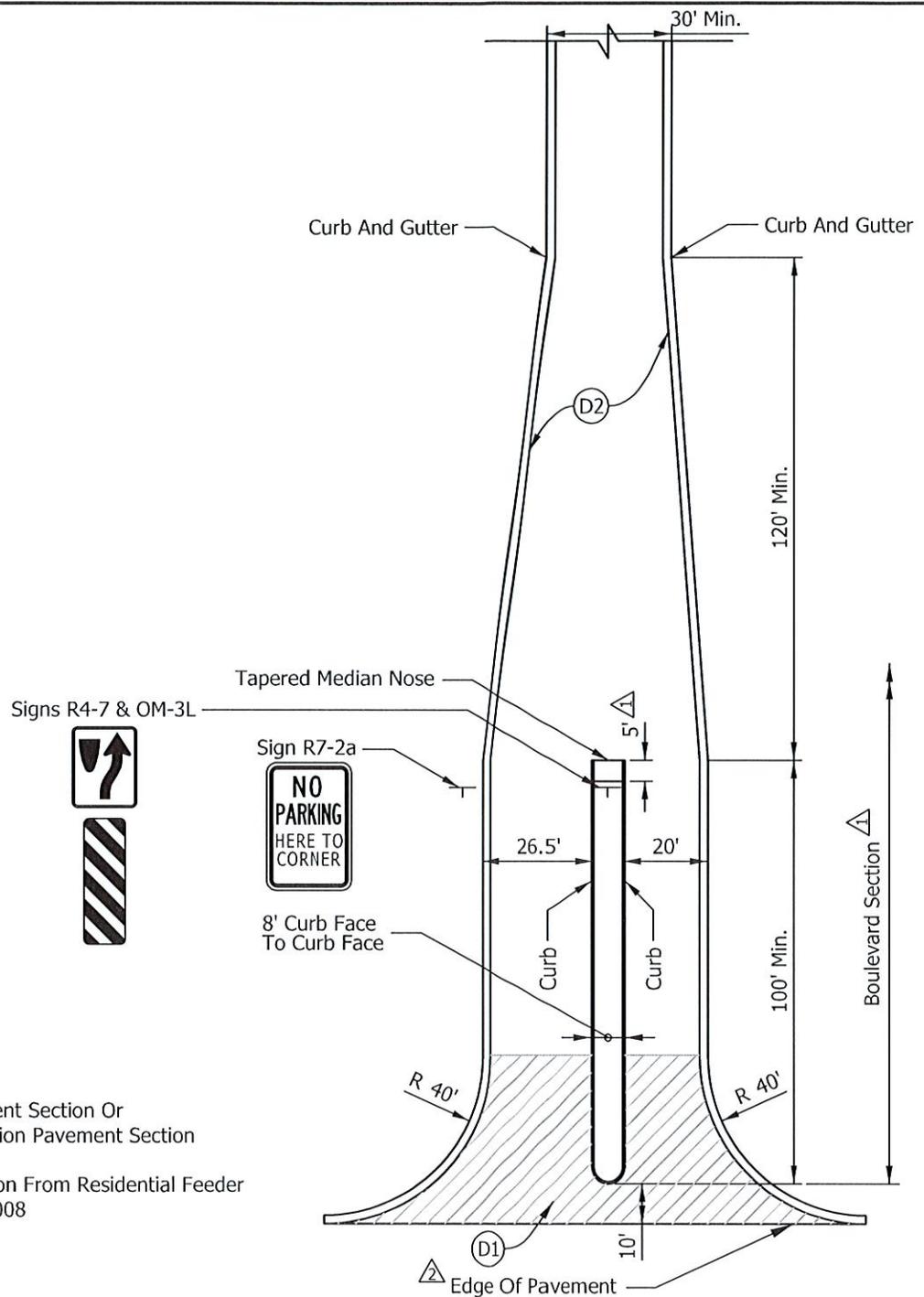
STANDARD DRAWING

NO. BC-603-005

SHEET 1 OF 1

SCALE: NO SCALE

3/27/2019 7:24:06 AM I:\P\170004 - Boone County As-Needed Assistance\Standards Consolidation\09Road\04 Design files\02 DGN\Plans\603-006\_BC\_Sub Div Entrance\_Sht\_Typical.dgn



**LEGEND:**

- (D1) Match Existing Pavement Section Or Acceleration/Deceleration Pavement Section
- (D2) Match Pavement Section From Residential Feeder STD Drawing BC-400-008

**NOTES:**

1. For Boulevard Sections, Continue Median Section; Maximum Length Between Breaks In Medians Shall Be 600' Minimum Length Between Islands Shall Be 30'
2. For Major Subdivisions, Acceleration/Deceleration Taper To Be Used To Tie Into Existing, See STD Drawing BC-603-008
3. Vegetation Shall Not Obstruction Sight Line
4. All Sign Posts Shall Be Mounted On Breakaway Anchors.

**BOONE COUNTY HIGHWAY DEPARTMENT**



**TYPICAL SUBDIVISION ENTRANCE**

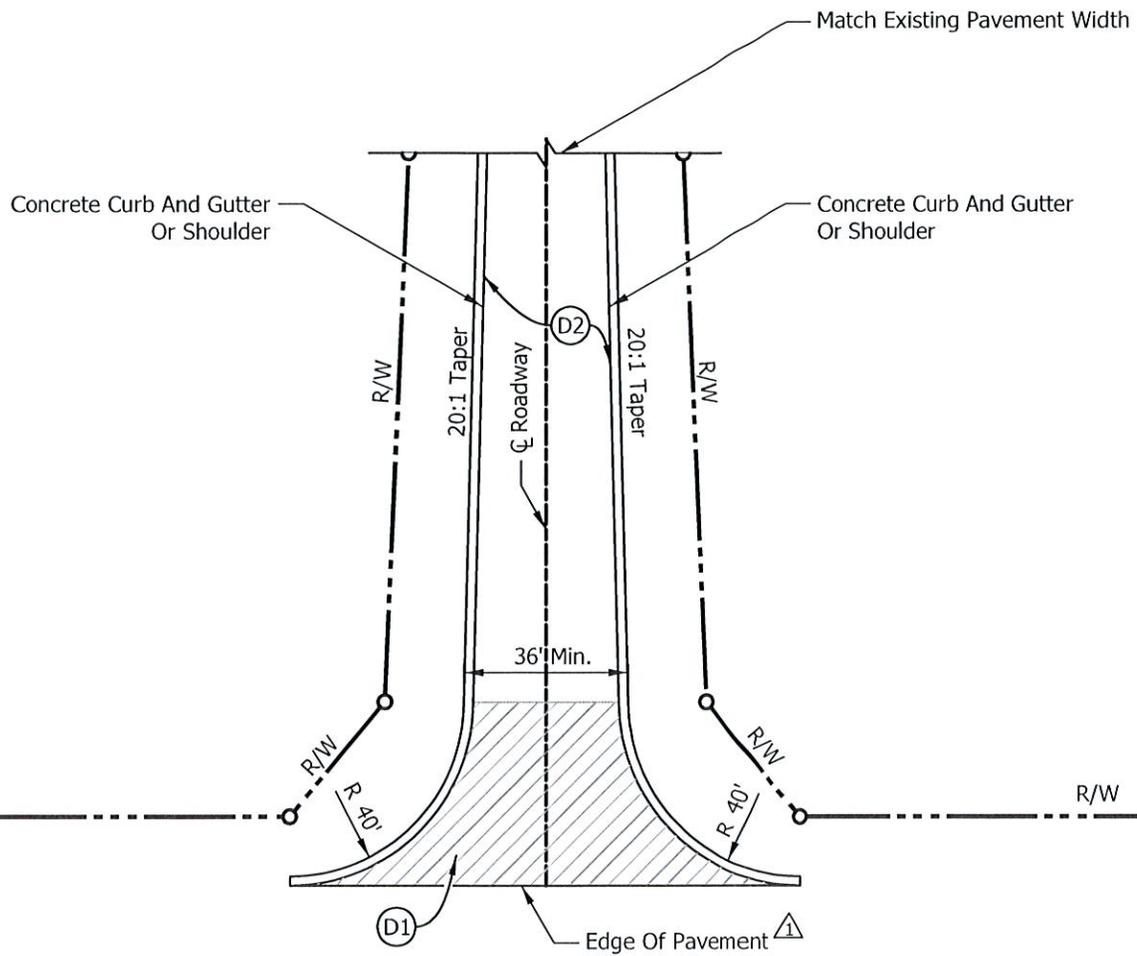
STANDARD DRAWING

NO. BC-603-006

SHEET 1 OF 1

SCALE: NO SCALE

3/27/2019 7:24:06 AM I:\P\170004 - Boone County As-Needed Assistance\Standards Consolidation\09Road\04 Design files\02 DGN\Plans\6005\603-007\_BC\_Street\_Approach\_Sht\_Typical.dgn



**LEGEND:**

- (D1) Match Existing Roadway Pavement Section
- (D2) Match Pavement Section From Residential Feeder STD Drawing BC-400-008

**NOTES:**

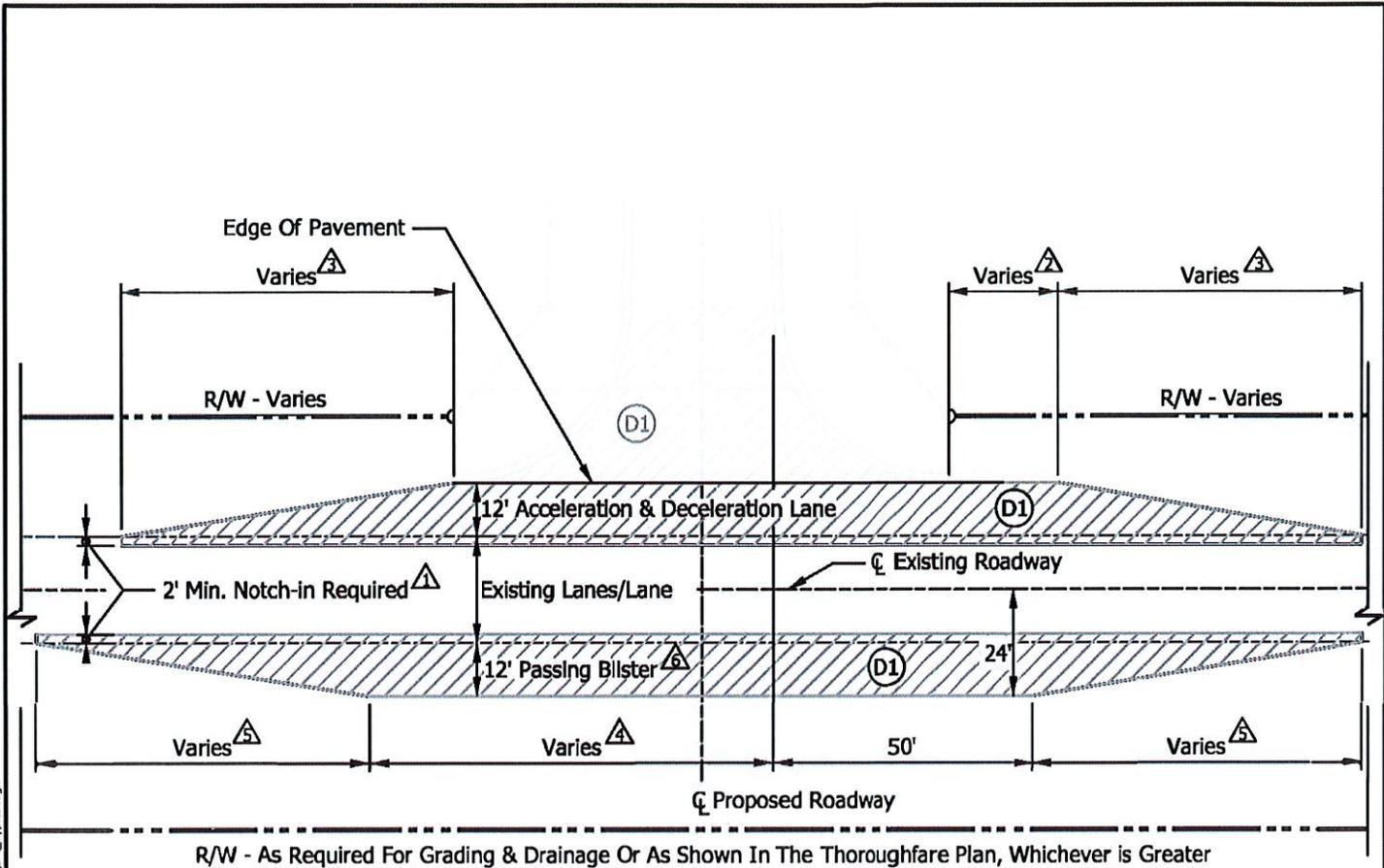
- △ Acceleration/Deceleration Taper To Used To Tie Into Existing, See STD Drawing BC-603-008

**BOONE COUNTY HIGHWAY DEPARTMENT**



**TYPICAL STREET APPROACH**

STANDARD DRAWING
NO. BC-603-007
SHEET 1 OF 1
SCALE: NO SCALE



R/W - As Required For Grading & Drainage Or As Shown In The Thoroughfare Plan, Whichever is Greater

**NOTES:**

- △ Acceleration/Deceleration & Passing Blister To Be Notched Into Existing Pavement With A Minimum Of 2' Clean Saw Cut Edge
- △ Deceleration Tangent See Table
- △ Acceleration/Deceleration Taper See Table
- △ Passing Blister Variable Tangent See Table
- △ Passing Blister Tapers See Table
- △ Passing Blisters Required When Deemed Necessary By The County Engineer
- 7. Construction Plans Shall Include Centerline Profile Of The Existing Road Being Intersected By The Proposed Road. The Profile Shall Extend A Minimum Of 500' From The Entrance Centerline In Each Direction.

Speed (MPH)	△	△
30 Or Less	75'	125'
Greater Than 30 & Less Than 50	100'	150'
Greater Than 50	100'	200'

Speed (MPH)	△	△
30 Or Less	150'	150'
Greater Than 30 & Less Than 50	150'	200'
Greater Than 50	200'	300'

**LEGEND:**

(D1) Match Existing Roadway Pavement Section

**BOONE COUNTY HIGHWAY DEPARTMENT**

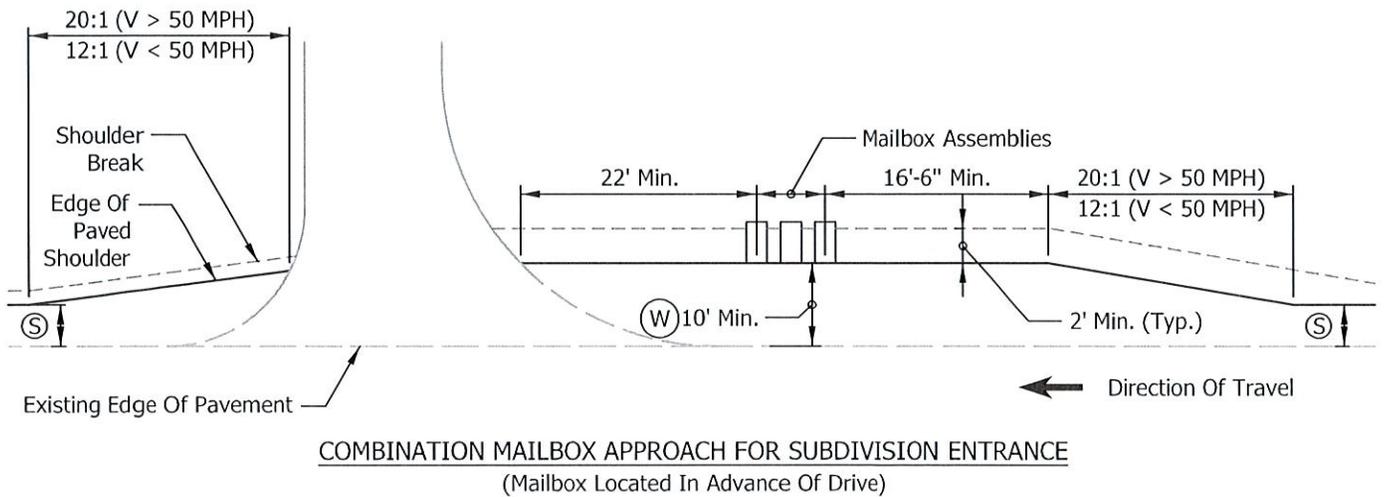
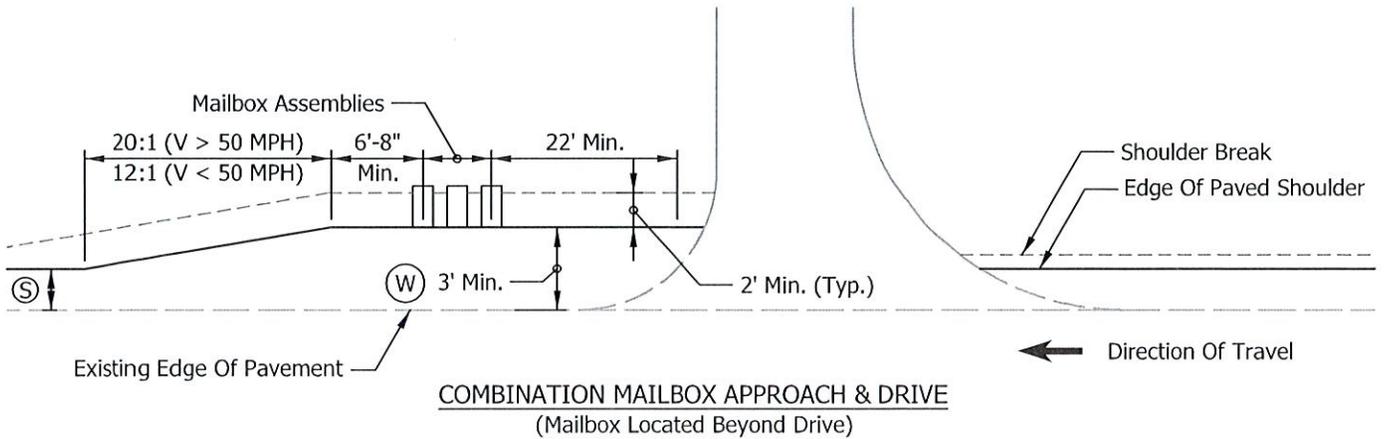
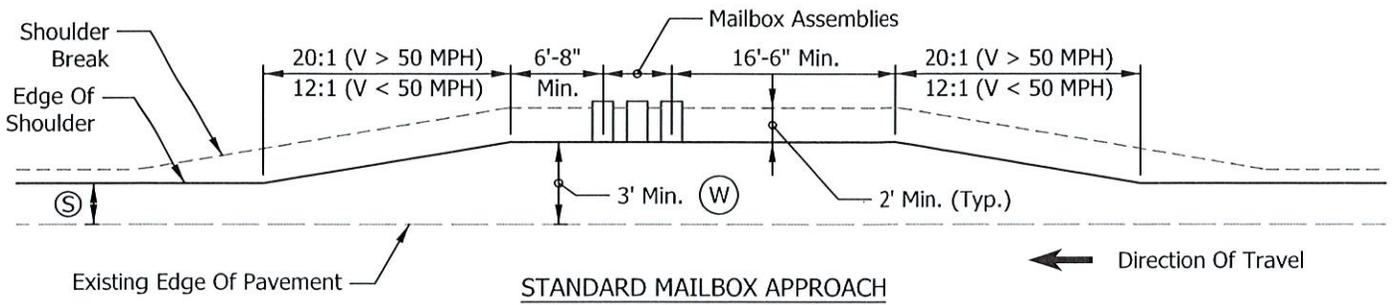
**TYPICAL ACCELERATION / DECELERATION LANE**

STANDARD DRAWING
NO. BC-603-008
SHEET 1 OF 1
SCALE: NO SCALE



3/27/2019 7:24:07 AM I:\P\170004 - Boone County As-Needed Assistance\Standards Consolidation\099\0401 Design files\02 DGN\Plans\603-008-003\_BC\_Accel Decel Taper\_Sht\_Typical.dgn

3/27/2019 7:24:07 AM I:\P\170004 - Boone County As-Needed Assistance\Standards Consolidation\09Road\04 Design files\02 DGN\Plans\600s\603-009\_BC\_Mailbox Approach\_Sht\_Typical.dgn



**LEGEND:**

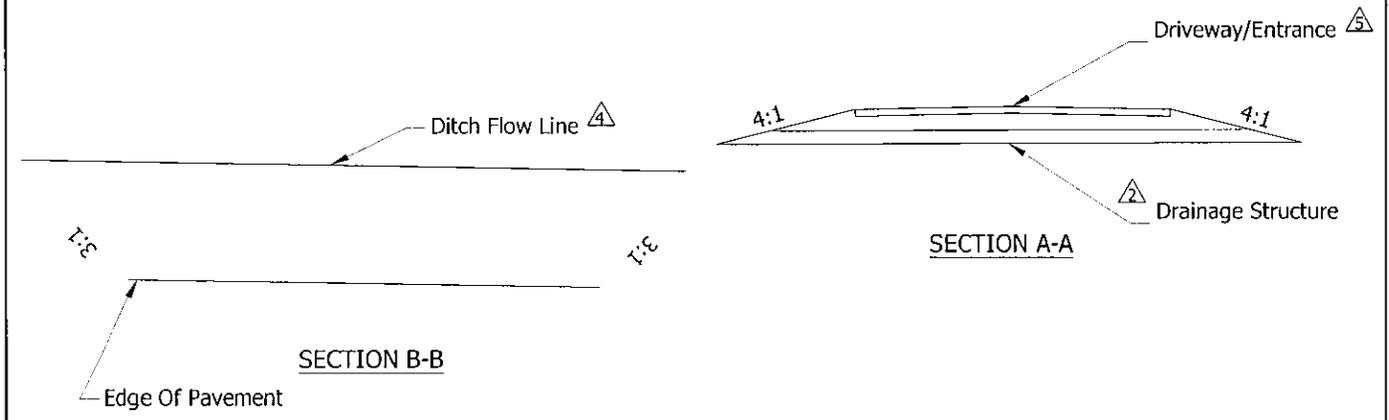
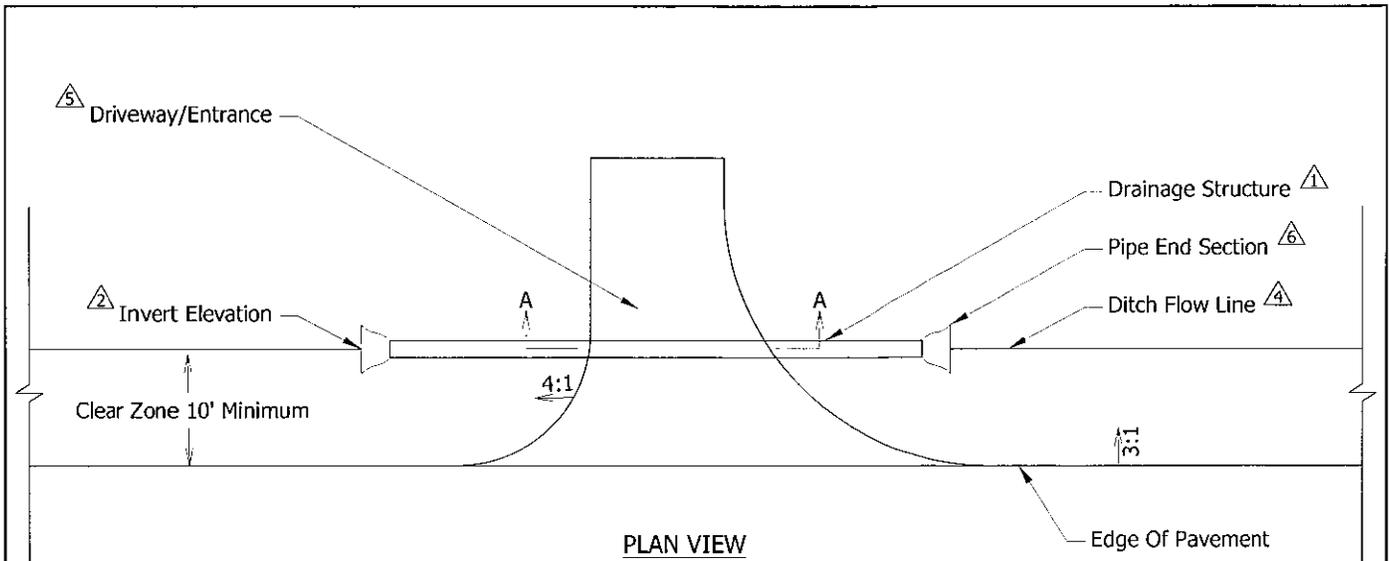
- (W) Match Existing Adjacent Pavement Section (Shoulder Or Lane)
- (S) Shoulder Width From Plans

**BOONE COUNTY HIGHWAY DEPARTMENT**



**TYPICAL MAILBOX APPROACH**

STANDARD DRAWING
NO. BC-603-009
SHEET 1 OF 1
SCALE: NO SCALE



**NOTES:**

- △ 12" Min. Diameter Structure (15" Diameter Desirable)  
Or As Directed By Boone County Engineer, Minimum 12" Cover
- △ Invert Elevations Shall Be Provided At All Downstream And  
Upstream Inverts.
- 3. Any And All Necessary Ditch And Drainage Improvements Shall Be Provided By  
Individual Performing The Installation
- △ Flow Line Elevations Shall Be Provided At The Beginning, End,  
And Every Grade Break.
- △ For Subdivision Residential Driveway Details - See STD Drawing BC-603-001  
For Urban Commercial Driveway Details - See STD Drawing BC-603-002  
For Field Entrance Details - See STD Drawing BC-603-003  
For Rural Residential Drive - See STD Drawing BC-603-004
- △ Pipe End Sections Shall Be In Accordance With Clear Zone Criteria  
Outlined In Current Version Of AASHTO Roadside Design Guide  
Drainage Structure Should Be A Min of 10' From Edge Of Pavement

REVISED MAY 2021

**BOONE COUNTY HIGHWAY DEPARTMENT**

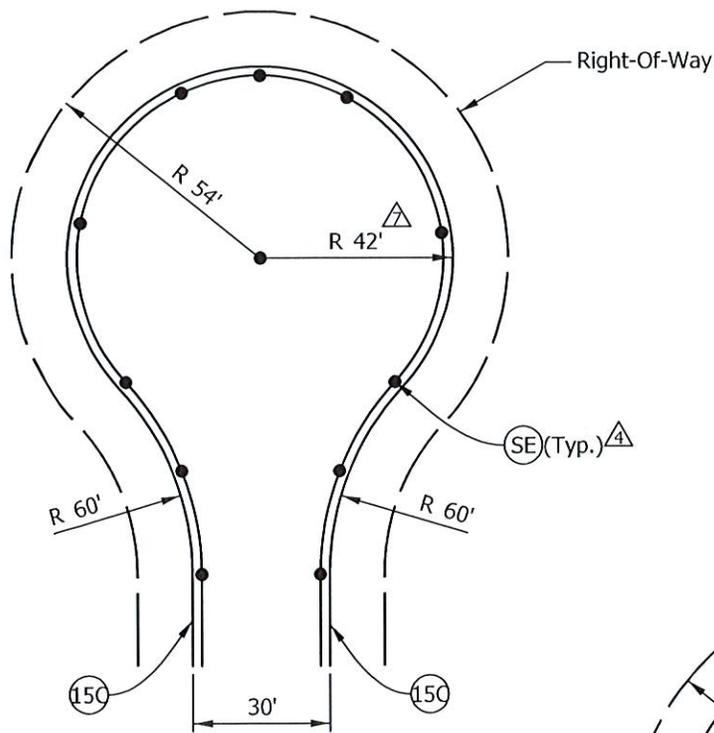


**TYPICAL CULVERT DETAIL WITH OPEN DITCHES**

STANDARD DRAWING
NO. BC-603-010
SHEET 1 OF 1
SCALE: NO SCALE

STATES - STIMES - SHEETS

3/27/2019 7:24:08 AM I:\P\170004 - Boone County As-Needed Assistance\Standards Consolidation\09Road\04 Design files\02 DGN\Plans\600s\604-001\_BC\_Sub Div Cul-De-Sac\_Sht\_Typical.dgn



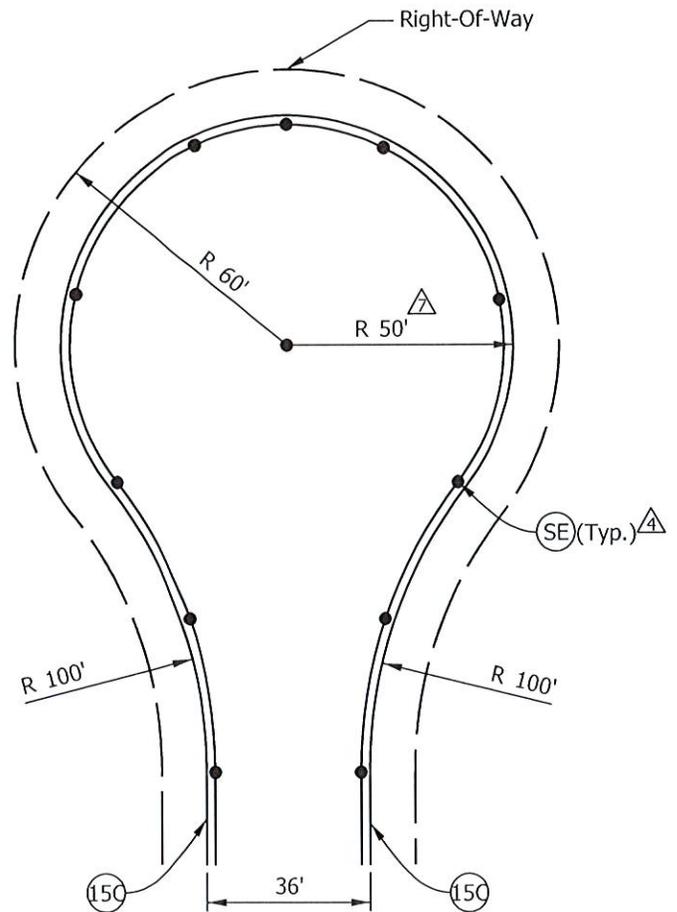
**TYPE-1 (CUL-DE-SAC)**

**NOTES:**

1. Type-1 Shall Be Used Only In Residential Subdivisions, All Others Shall Be Type-2.
2. Warning Signs Shall Be Posted At The Street Entrance, Indicating No Outlet.
3. One Detail Shall Be Provided For Each Cul-De-Sac. Each Detail Shall Be Identified By Street Name.
- △ Elevations Provided Shall Be Proposed Flow Line Of Gutter.
5. Street Width And Radius Lengths Are Measured To The Back Of Curb.
6. No Center Islands Permitted.
- △ Radius Shall Be In Accordance With Current Fire Protection Ordinance. May Differ From Dimensions Shown.

**LEGEND:**

- (150) Rolled Curb, See STD Drawing BC-601-001
- (SE) Spot Elevations To Be Provided At Locations As Described Below:  
Beginning, Middle, And End Of Curves Approximately 40' Spacing  
Along Cul-De-Sac Edge Of Pavement Center Of Cul-De-Sac (High-Point)



**TYPE-2 (CUL-DE-SAC)**

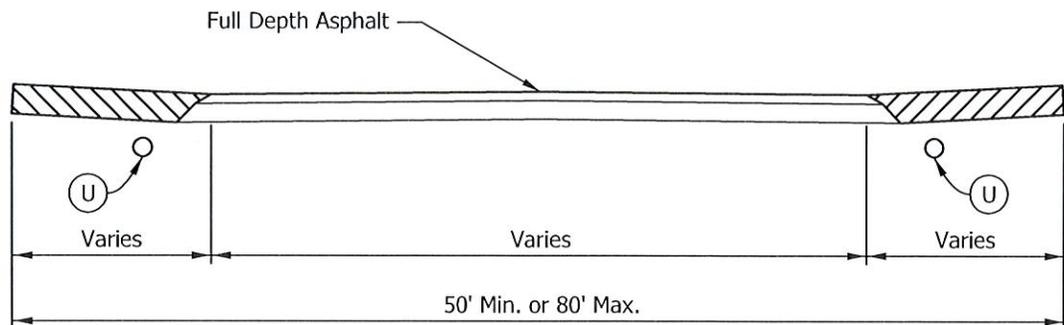
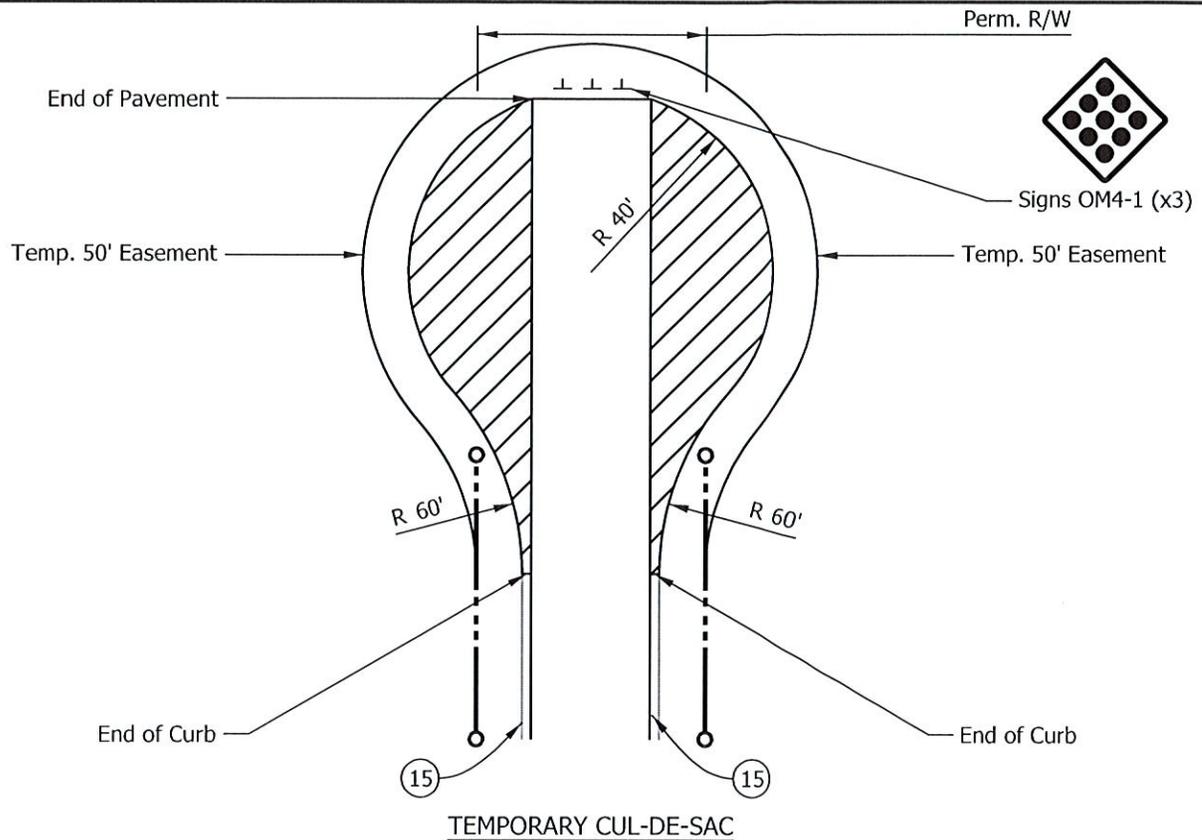
**BOONE COUNTY HIGHWAY DEPARTMENT**



**CUL-DE-SAC**

STANDARD DRAWING
NO. BC-604-001
SHEET 1 OF 1
SCALE: NO SCALE

3/27/2019 7:24:08 AM I:\P\170004 - Boone County As-Needed Assistance\Standards Consolidation\Road\04 Design files\02 DGN\Plans\600s\604-002\_BC\_Temp\_Sub Div Cul-De-Sac\_Typical.dgn



TEMPORARY CUL-DE-SAC TYPICAL

LEGEND

- (15) Concrete Curb And Gutter - See STD Drawing BC-601-001
- (U) Underdrain (Typ.) - See STD Drawing BC-401-001
- Temporary Pavement To Be Removed When Street Is Continued. Thickness And Type To Match Permanent Pavement.

BOONE COUNTY HIGHWAY DEPARTMENT



TEMPORARY CUL-DE-SAC

STANDARD DRAWING
NO. BC-604-002
SHEET 1 OF 1
SCALE: NO SCALE