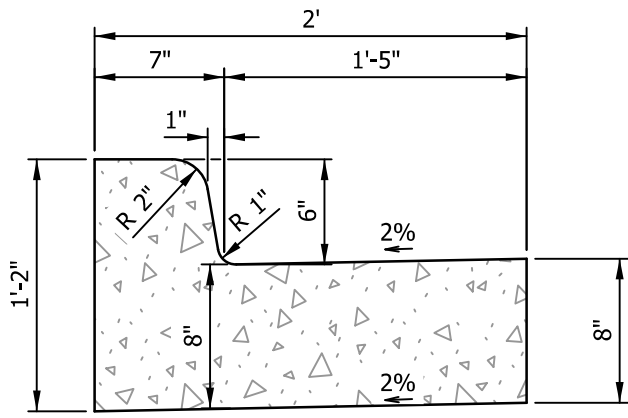


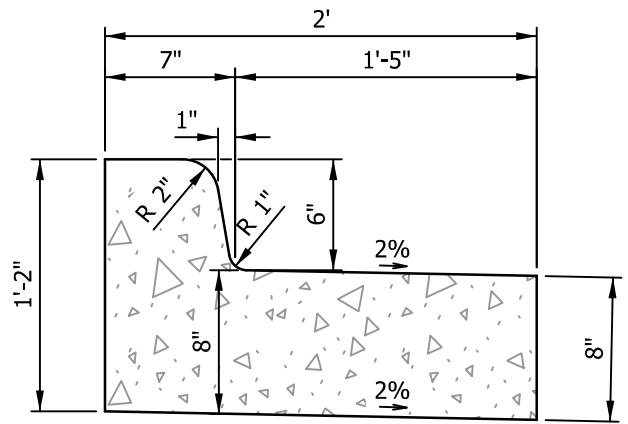
INDEX OF BOONE COUNTY STANDARDS
600 SERIES STANDARD DRAWINGS
INCIDENTAL CONSTRUCTION

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Typical Concrete Sidewalk Detail.....	BC-602-001
Typical HMA Multi-Use Path.....	BC-602-002
Typical Subdivision Residential Drive.....	BC-603-001
Typical Urban Commercial Drive.....	BC-603-002
Typical Field Entrance.....	BC-603-003
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Typical Culvert Detail With Open Ditches.....	BC-603-010
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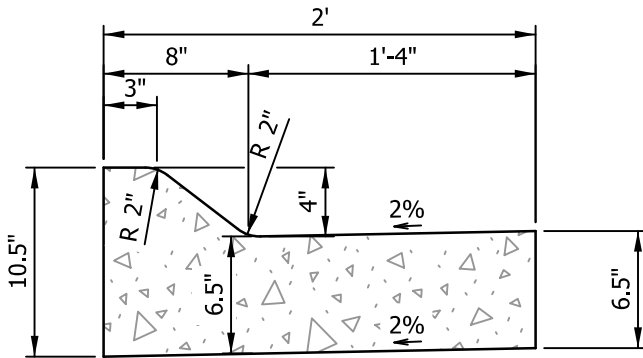
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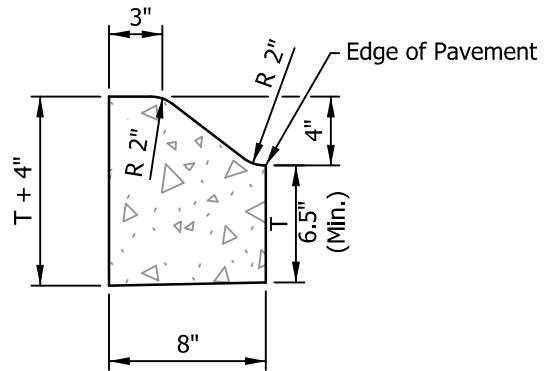
**CONCRETE CURB AND GUTTER
STANDARD (Typ.)**



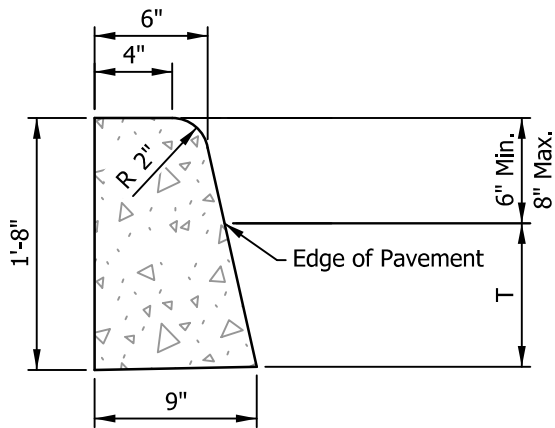
**CONCRETE CURB AND GUTTER
MODIFIED (Typ.)**



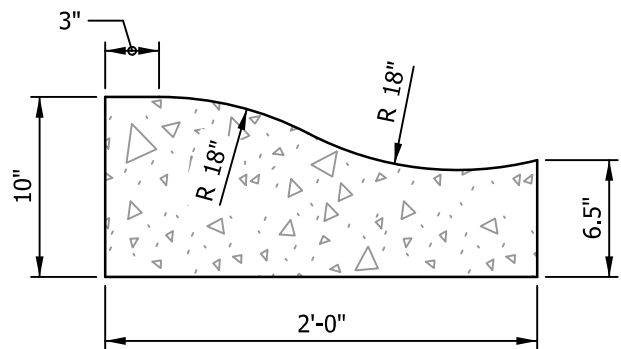
**CONCRETE CURB AND GUTTER
MOUNTABLE (Typ.)**



**CONCRETE CURB
MOUNTABLE**



**CONCRETE CURB
BARRIER**



ROLLED CURB

NOTES

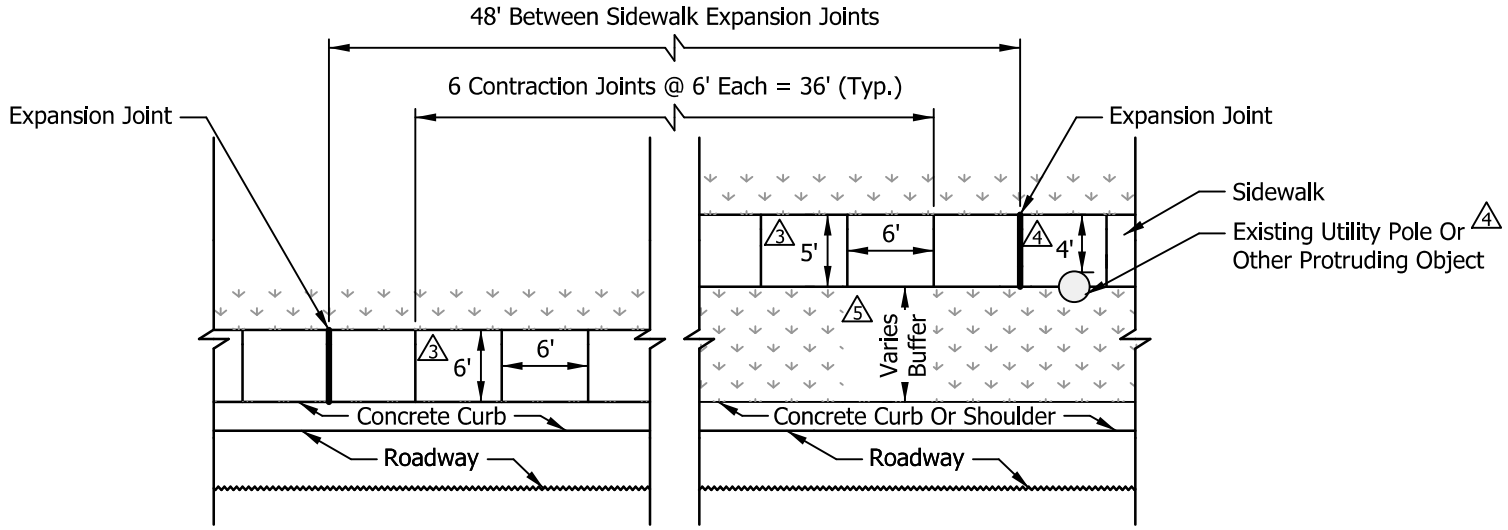
T = Nominal Pavement Thickness

BOONE COUNTY HIGHWAY DEPARTMENT

**CONCRETE CURB AND GUTTER
AND CURB**



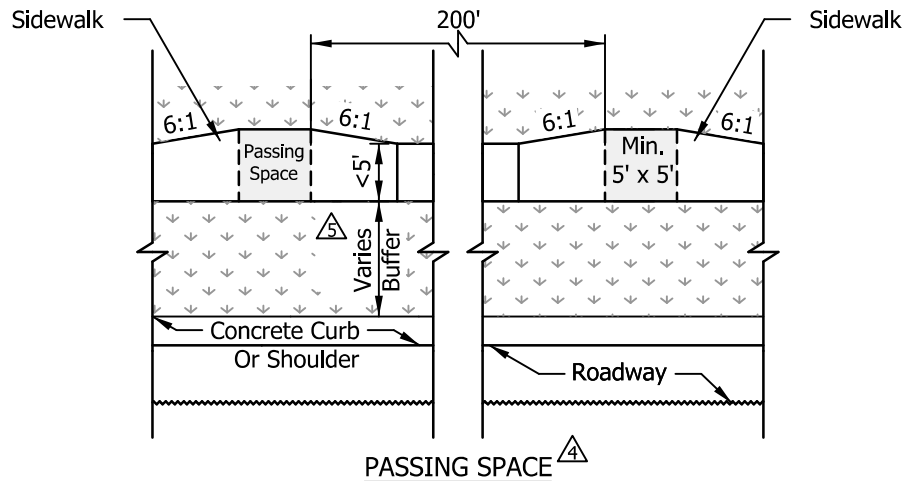
STANDARD DRAWING
NO. BC-601-001
SHEET 1 OF 1
SCALE: NO SCALE



SIDEWALK PLAN VIEW

NOTES:

1. All Slopes Are Absolute Rather Than Relative To The Sidewalk Or Roadway Grade And Are Given As Maximums. Slopes At Least 0.50% Less Than The Maximum Are Preferred.
 2. The Grade Of The Sidewalk Is Measured In The Direction Of Pedestrian Travel And Shall Not Exceed The Grade Of The Adjacent Roadway. The Cross Slope Is Measured Perpendicular To The Direction Of Pedestrian Travel And Shall Not Exceed 2.00%
- ⚠ Where There Is A Buffer Between The Sidewalk And The Curb, The Preferred Minimum Sidewalk Clear Width Is 5'. Where There Is No Buffer Between The Sidewalk And The Curb, The Preferred Minimum Sidewalk Width Is 6'.
- ⚠ A 4' Minimum Clear Width Shall Be Provided Adjacent To Street Furniture, Mailbox, Utility Pole, Or Other Protruding Object. Where The Sidewalk Clear Width Is Less Than 5', A Passing Space With Dimensions Of 5' X 5' Shall Be Provided At 200' Intervals
- ⚠ See Roadway Typical Sections for Dimensions

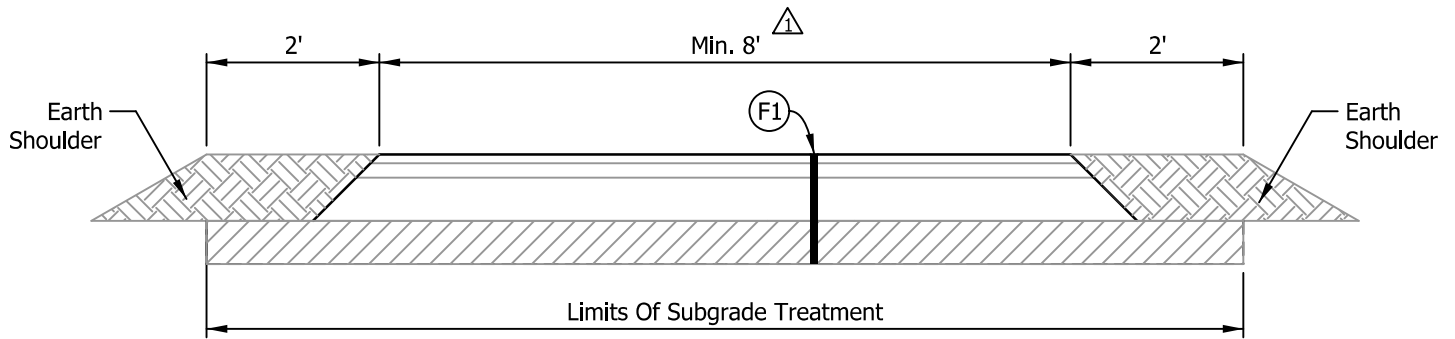


PASSING SPACE

LEGEND:

- (F) 4" Sidewalk, Concrete On
4" Compacted Aggregate No. 53

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LEGEND:

NOTES:

- (F1) 1.5" HMA Surface, 9.5mm On
- 2.5" HMA Intermediate, 19.0mm On
- 6.0" Compacted Aggregate. No. 53 On
- Typ. INDOT Subgrade Treatment Type III

- △ Width Varies, Typ. 8' - 12'
- Cross Slope Varies, Typ. Max. 2.0%

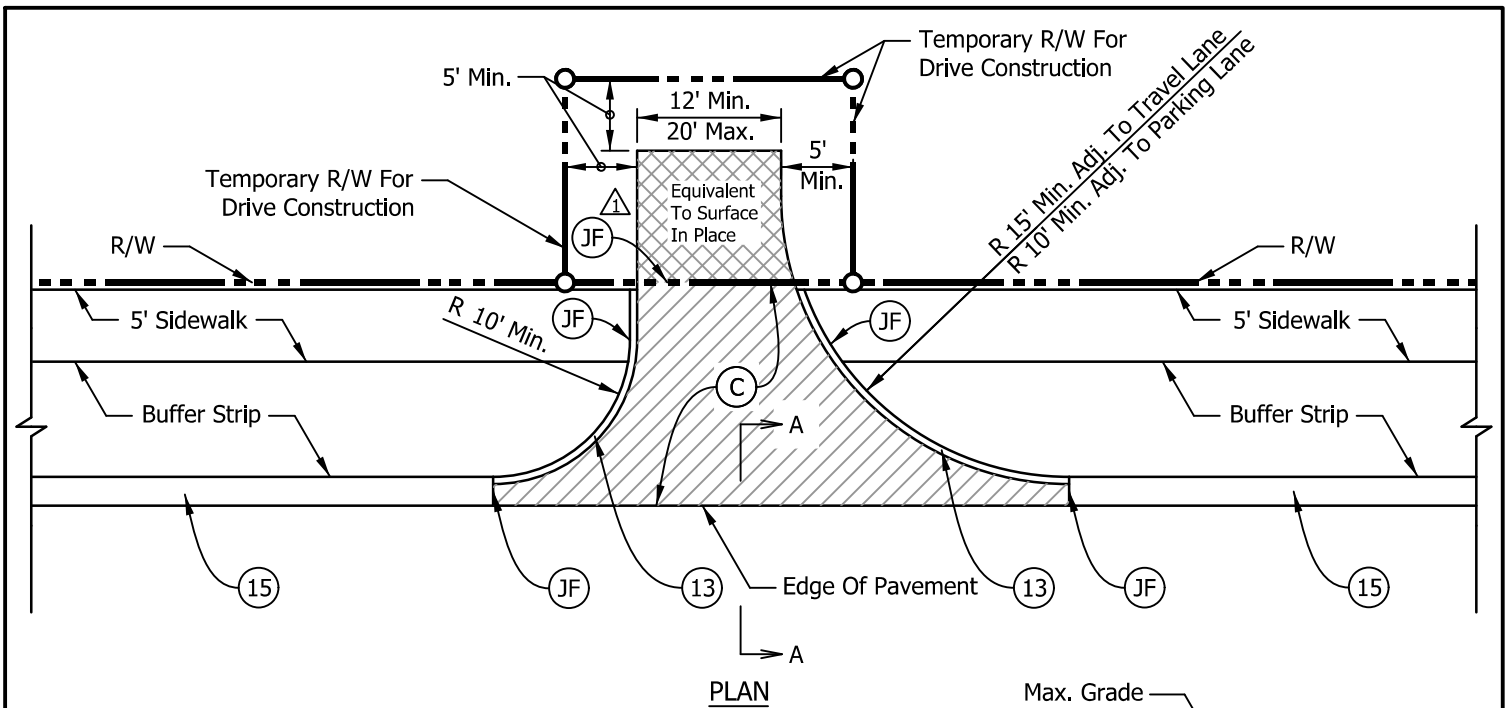
BOONE COUNTY HIGHWAY DEPARTMENT



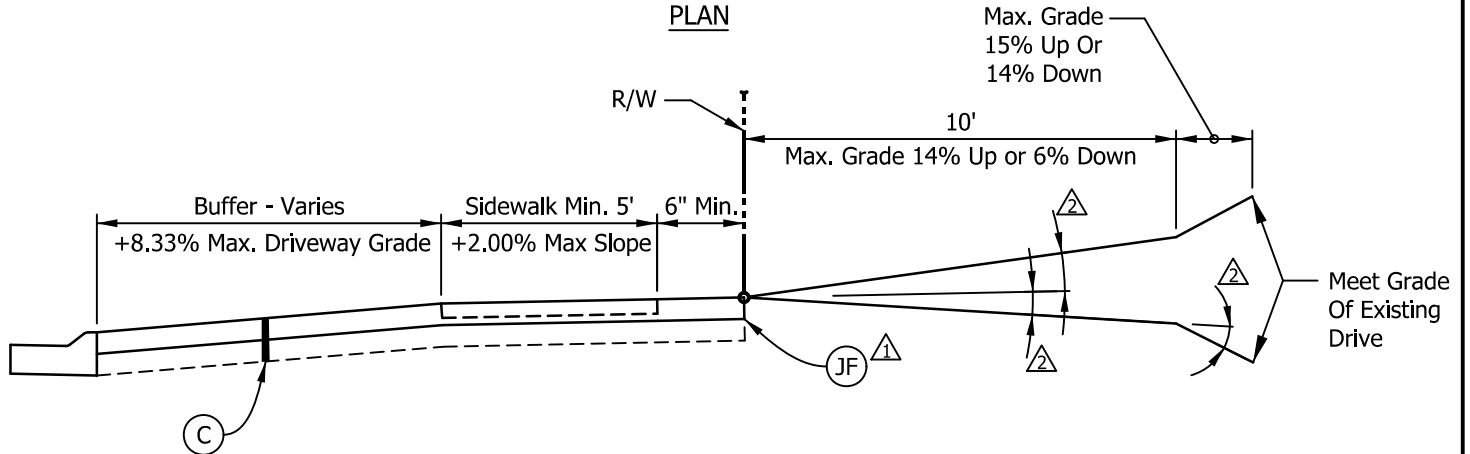
TYPICAL HMA MULTI-USE PATH

STANDARD DRAWING
NO. BC-602-002
SHEET 1 OF 1
SCALE: NO SCALE

3/27/2019 7:24:03 AM - Boone County As-Needed Assistance\Standards Consultation\09Road\04 Design Files\02 DGN\Plans\6005\603-001_BC_Res_Drive_Sht_Typical.dgn



PLAN



SECTION A-A

LEGEND:

NOTES:

(C)	Pavement Layer	Asphalt	Concrete	Aggregate
	HMA Surface, Type B	-	-	-
	HMA Intermediate, Type B	-	-	-
	HMA Base, Type B	-	-	-
	Compacted Aggregate No. 53	-	-	-
	INDOT Subgrade Treatment	-	Type II	8"
	PCCP for Approaches	N/A	6"	N/A

- Joint Filler Required When Existing Drive Is PCCP
- Max. Algebraic Diff. In Grades; Crests 8.00% Sags 12.00%
- 8" Compacted Aggregate May Be Substituted For PCCP With Approval From County Engineer
- 4. Sidewalk Elevation Transitions At Drives To Be In Compliance With Current ADA Guidelines
- 5. Driveway Culverts Which Cannot Be Located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards
- 6. For Drainage Structure Details See STD Drawing BC-603-010

- (JF) 1/2" Preformed Joint Filler
- (13) Monolithic Curb See STD Drawing BC-601-001
- (15) Concrete Curb And Gutter See STD Drawing BC-601-001

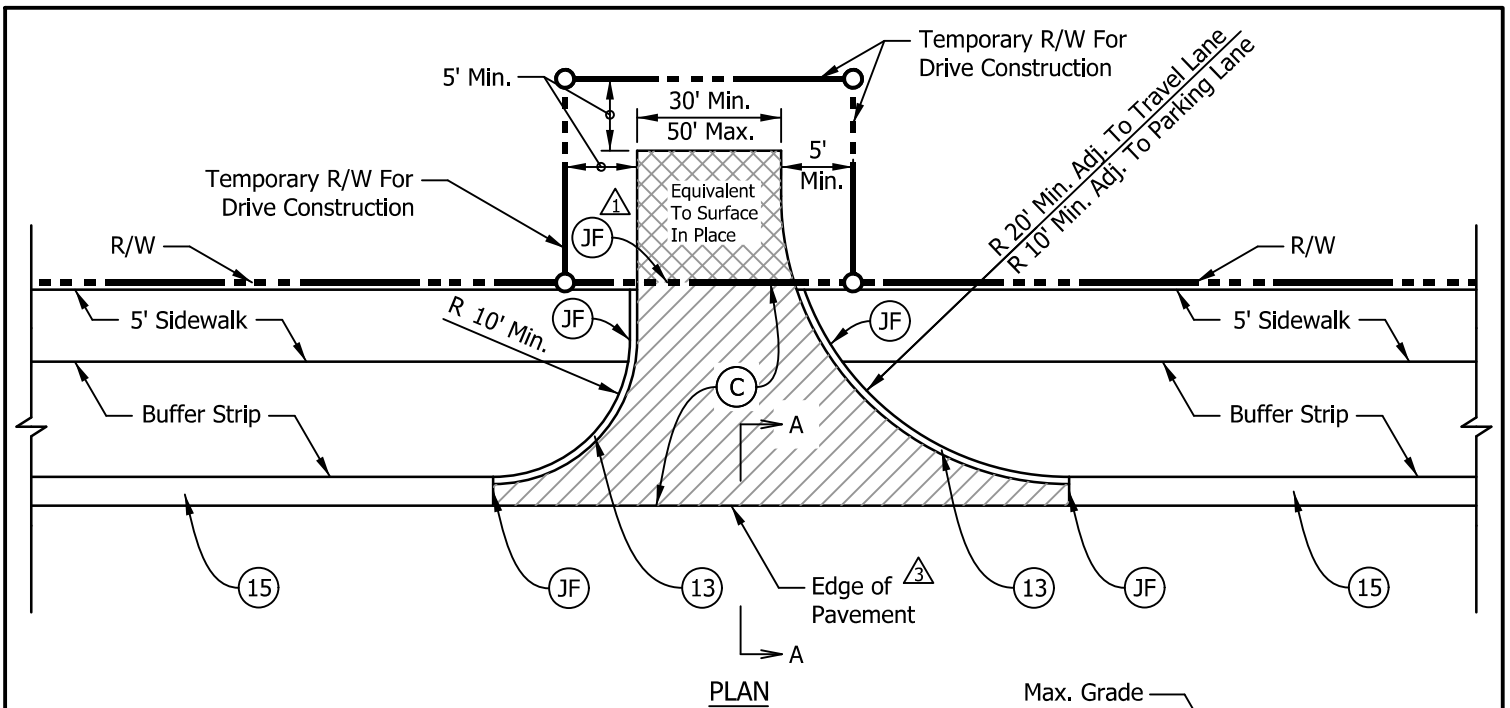
BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL SUBDIVISION RESIDENTIAL DRIVEWAY

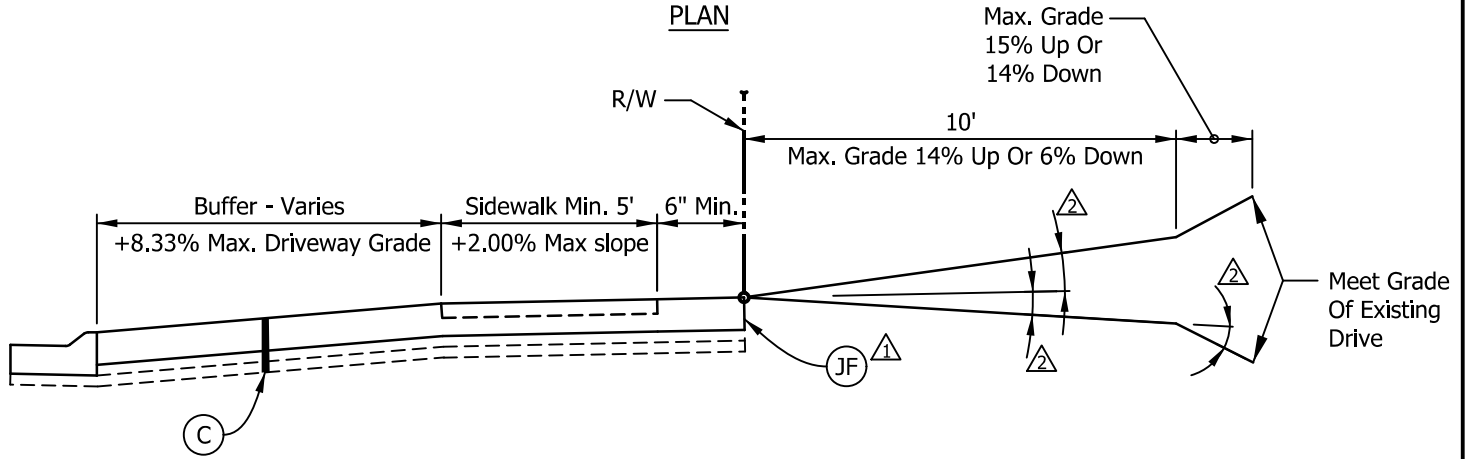


STANDARD DRAWING
NO. BC-603-001
SHEET 1 OF 1
SCALE: NO SCALE

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PLAN



SECTION A-A

LEGEND:

NOTES:

(C)	Pavement Layer	Asphalt	Concrete	Aggregate
	HMA Surface, Type B	-	-	-
	HMA Intermediate, Type B	-	-	-
	HMA Base, Type B	-	-	-
	Compacted Aggregate No. 53	-	-	-
	INDOT Subgrade Treatment	-	Type II	-
	PCCP for Approaches	N/A	9"	N/A

- (JF) 1/2" Preformed Joint Filler
- (13) Monolithic Curb See STD Drawing BC-601-001
- (15) Concrete Curb And Gutter See STD Drawing BC-601-001

- △ Joint Filler Required When Existing Drive Is PCCP
- △ Max. Algebraic Diff. In Grades;
Crests 8.00%
Sags 12.00%
- △ If Warranted By County Engineer Acceleration/
Deceleration Taper To Be Added,
See STD Drawing BC-603-008
- 4. Sidewalk Elevation Transitions At Drives To Be
In Compliance With Current ADA Guidelines
- 5. Driveway Culverts Which Cannot Be located
Outside Of The Clear Zone Shall Require Grated
End Sections In Accordance With INDOT Standards
- 6. Commercial Drives Shall Be Required At
Truck Stops And Entrances To Heavy
Industrial Properties
- 7. For Drainage Structure Details
See STD Drawing BC-603-010

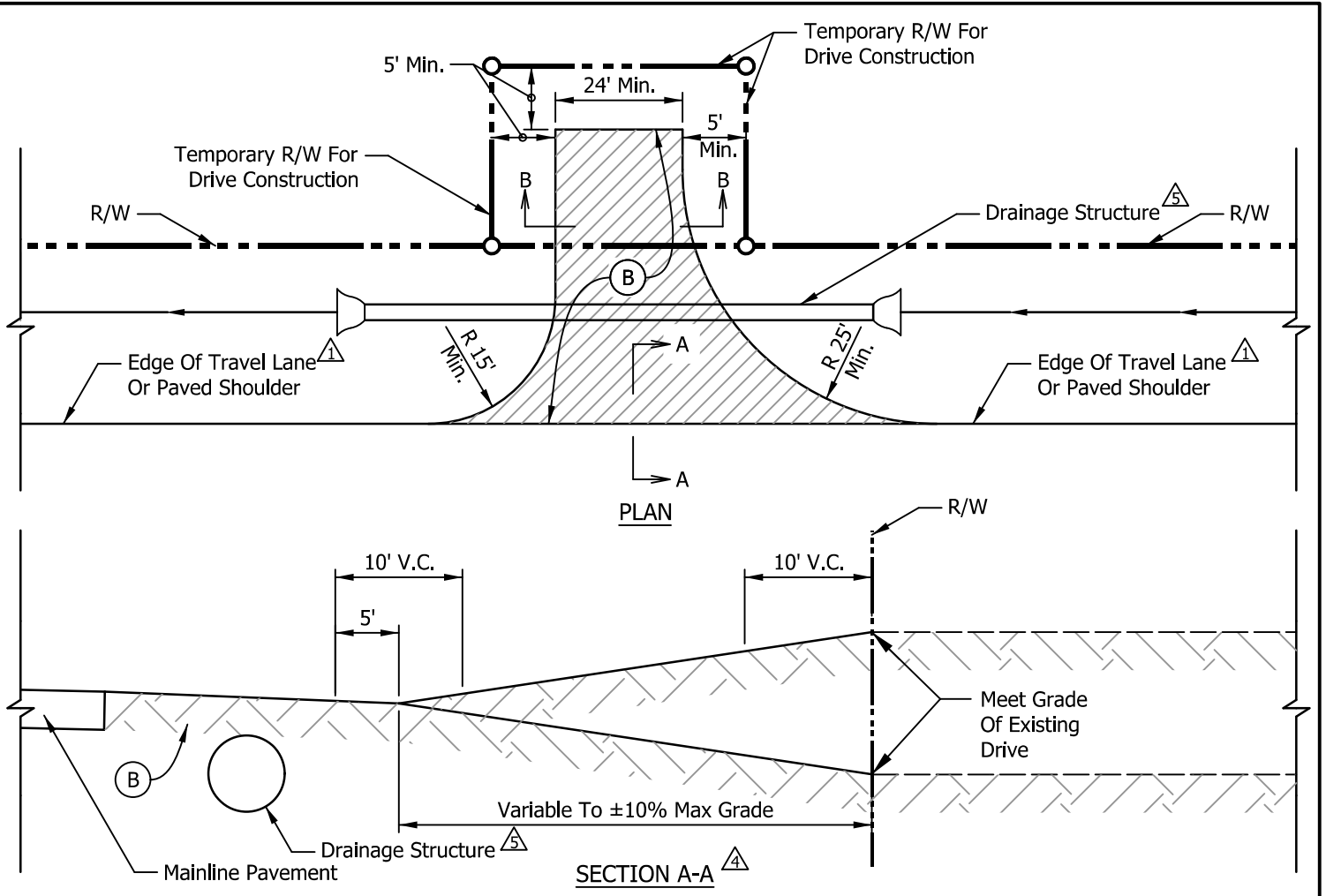
BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL URBAN COMMERCIAL DRIVEWAY

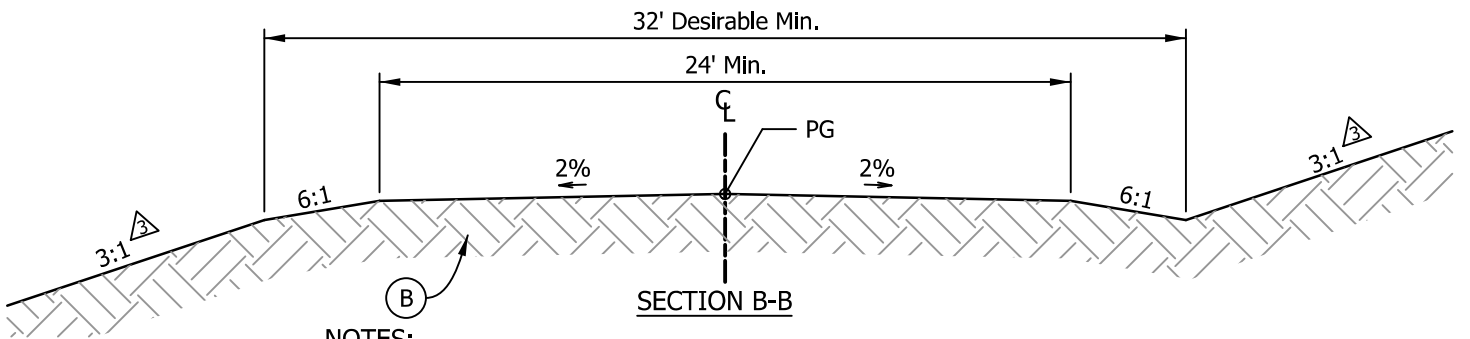


STANDARD DRAWING
NO. BC-603-002
SHEET 1 OF 1
SCALE: NO SCALE

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SECTION A-A



SECTION B-B

NOTES:

- 1. Where The Shoulder Is Earth Or Aggregate, Or When The Paved Shoulder Is Less Than 8', The Drive Radii Shall Be Tangent To The Travel Lane. Where The Paved Shoulder Is 8' Or More, The Drive Radii Shall Be Tangent To The Paved Shoulder
- 2. Driveway Culverts Which Cannot Be located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards
- 3. Foreslopes And Backslopes Shall Be In Accordance With Clear Zone Requirements Set Forth By INDOT Design Manual (Current Edition)
- 4. Minimum 2% Grade For 10' Away From Edge Of Pavement Required For Gravel Drives
- 5. For Drainage Structure Details - See STD Drawing BC-603-010

LEGEND:

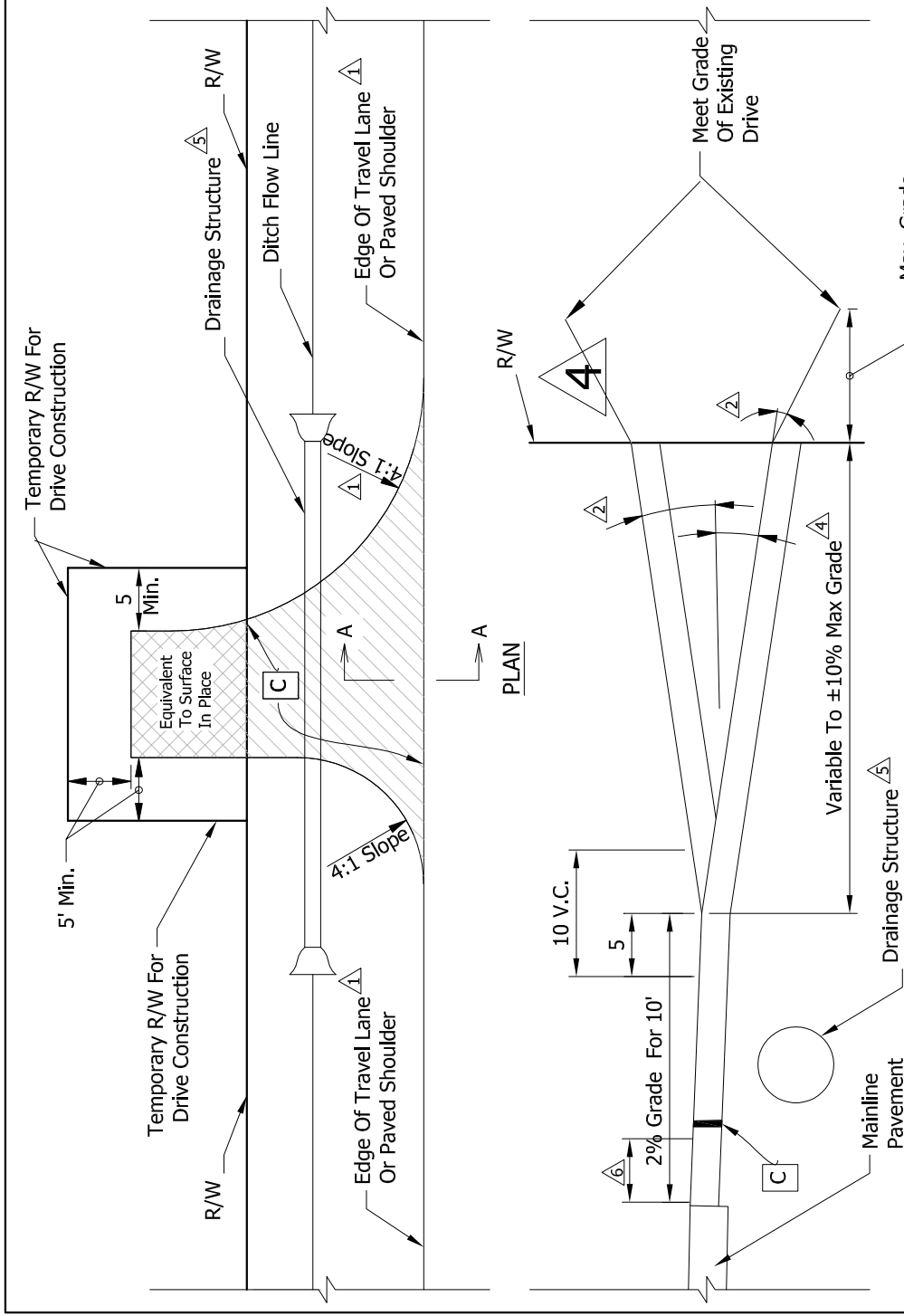
(B) Earth Fill Or Aggregate As Needed

BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL FIELD ENTRANCE



STANDARD DRAWING
NO. BC-603-003
SHEET 1 OF 1
SCALE: NO SCALE



SECTION A-A

LEGEND:

Pavement Layer	Asphalt	Concrete	Aggregate	Mainline Resurfacing
HMA Surface, Type B	1.5"	-	-	per plan
HMA Intermediate, Type B	3.5"	-	-	per plan
HMA Base, Type B	-	-	-	N/A
Compacted Aggregate No. 53	-	-	6"	N/A
INDOT Subgrade Treatment	Type II	Type II	Type II	N/A
PCCP for Approaches	N/A	6"	N/A	N/A
Minimum Width for HMA Approach	N/A	2'	N/A	2'

NOTES:

- 1 Where The Shoulder Is Earth Or Aggregate, Or When The Paved Shoulder Is Less Than 8', The Drive Radii Shall Be Tangent To The Travel Lane. Where The Paved Shoulder Is 8' Or More, The Drive Radii Shall Be Tangent To The Paved Shoulder
- 2 Max. Algebraic Diff. In Grades; Crests 8.00%, Sags 12.00%
- 3 Driveway Culverts Which Cannot Be Located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards. Drainage Structure Should Be Minimum Of 10' From Edge Of Pavement (Clear Zone)
- 4 Minimum 2% Grade For 10' Away From Edge Of Pavement Required For All Drives. 2.5" For Every 10' From Edge of Pavement
- 5 For Drainage Structure Details - See STD Drawing BC-603-010

REVISED MAY 2021

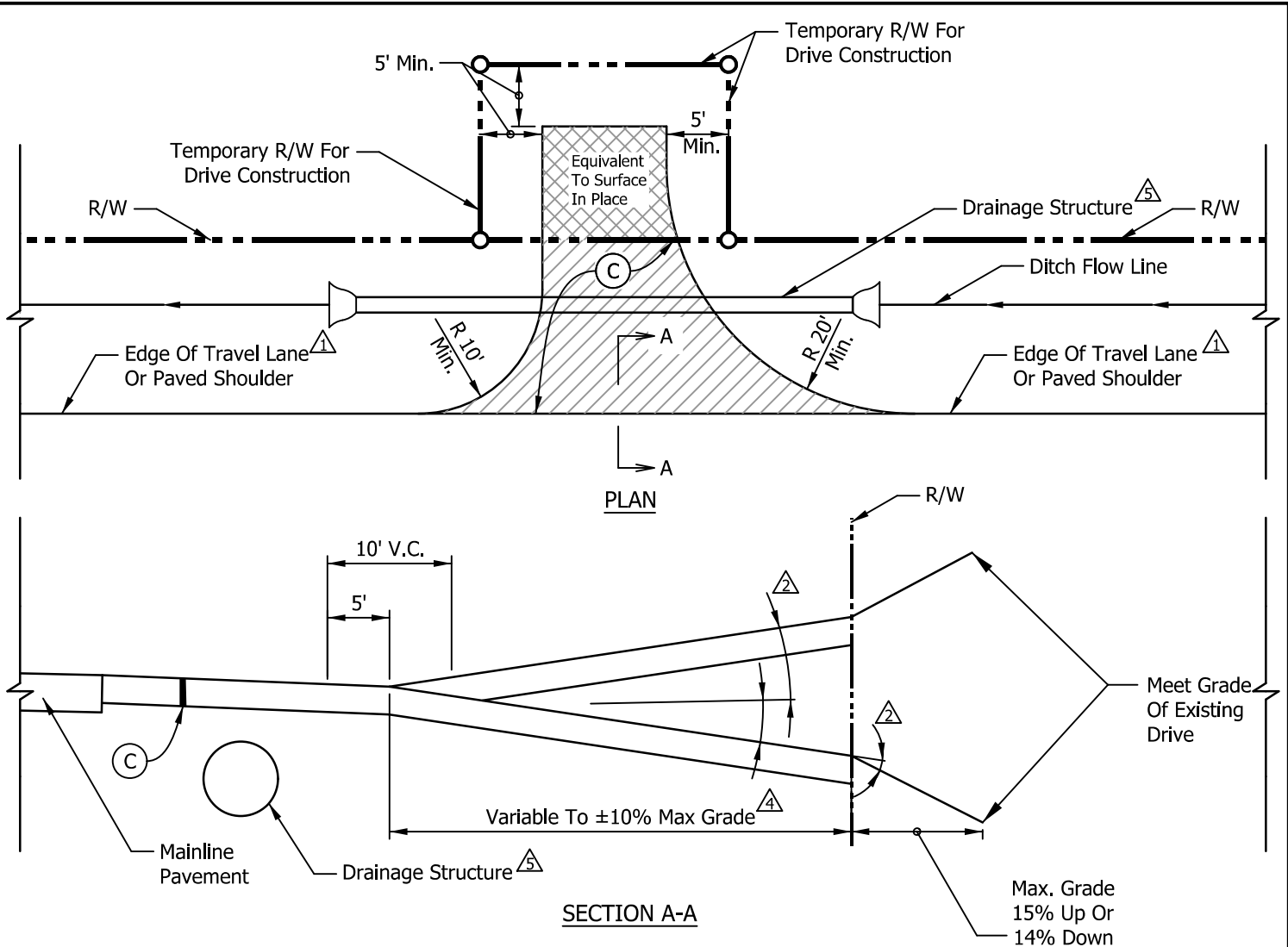
BOONE COUNTY
Highway Department

BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL RURAL RESIDENTIAL DRIVEWAY

STANDARD DRAWING
NO. BC-603-004
SHEET 1 OF 1
SCALE: NO SCALE

3/27/2019 7:24:05 AM I:\170004 - Boone County As-Needed Assistance\Standards Consultation\09Road\04 Design files\02 DGN\Plans\6003\603-005_Rural Commercial Drive_Sht_Typical.dgn



LEGEND:

(C)	Pavement Layer	Asphalt	Concrete	Aggregate
	HMA Surface, Type B	1.5"	-	-
	HMA Intermediate, Type B	2.5"	-	-
	HMA Base, Type B	8.0"	-	-
	Compacted Aggregate No. 53	-	-	12"
	INDOT Subgrade Treatment	Type II	Type II	Type II
	PCCP for Approaches	N/A	9"	N/A

NOTES:

- △ Where The Shoulder Is Earth Or Aggregate, Or When The Paved Shoulder Is Less Than 8', The Drive Radii Shall Be Tangent To The Travel Lane. Where The Paved Shoulder Is 8' Or More, The Drive Radii Shall Be Tangent To The Paved Shoulder
- △ Max. Algebraic Diff. In Grades; Crests 8.00%, Sags 12.00%
- 3. Driveway Culverts Which Cannot Be located Outside Of The Clear Zone Shall Require Grated End Sections In Accordance With INDOT Standards
- △ Minimum 2% Grade For 10' Away From Edge Of Pavement Required For Gravel Drives
- △ For Drainage Structure Details - See STD Drawing BC-603-010

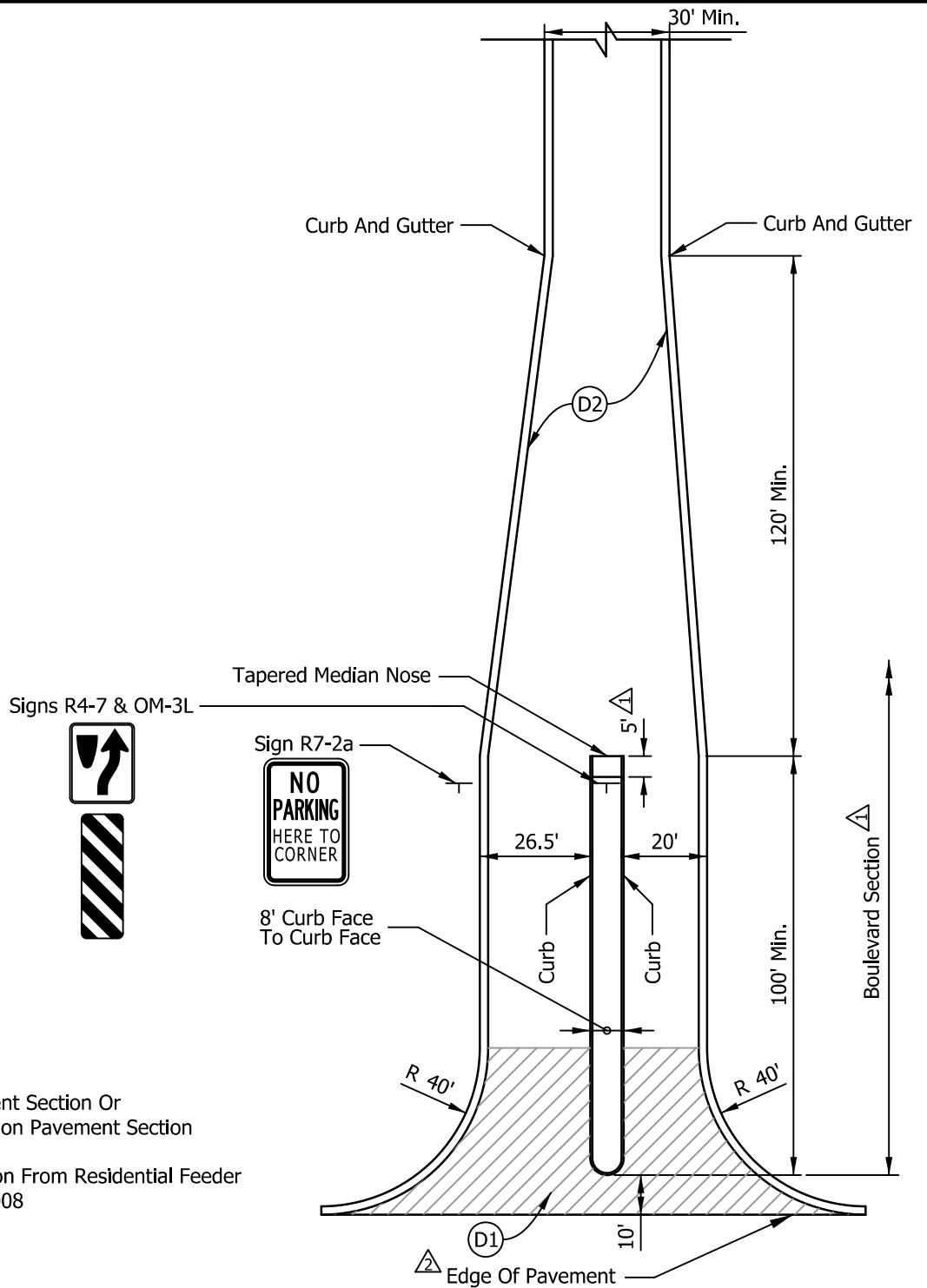
BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL RURAL COMMERCIAL DRIVEWAY

STANDARD DRAWING
NO. BC-603-005
SHEET 1 OF 1
SCALE: NO SCALE



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LEGEND:

- (D1) Match Existing Pavement Section Or Acceleration/Deceleration Pavement Section
- (D2) Match Pavement Section From Residential Feeder STD Drawing BC-400-008

NOTES:

- △ For Boulevard Sections, Continue Median Section; Maximum Length Between Breaks In Medians Shall Be 600' Minimum Length Between Islands Shall Be 30'
- △ For Major Subdivisions, Acceleration/Deceleration Taper To Be Used To Tie Into Existing, See STD Drawing BC-603-008
- 3. Vegetation Shall Not Obstruction Sight Line
- 4. All Sign Posts Shall Be Mounted On Breakaway Anchors.

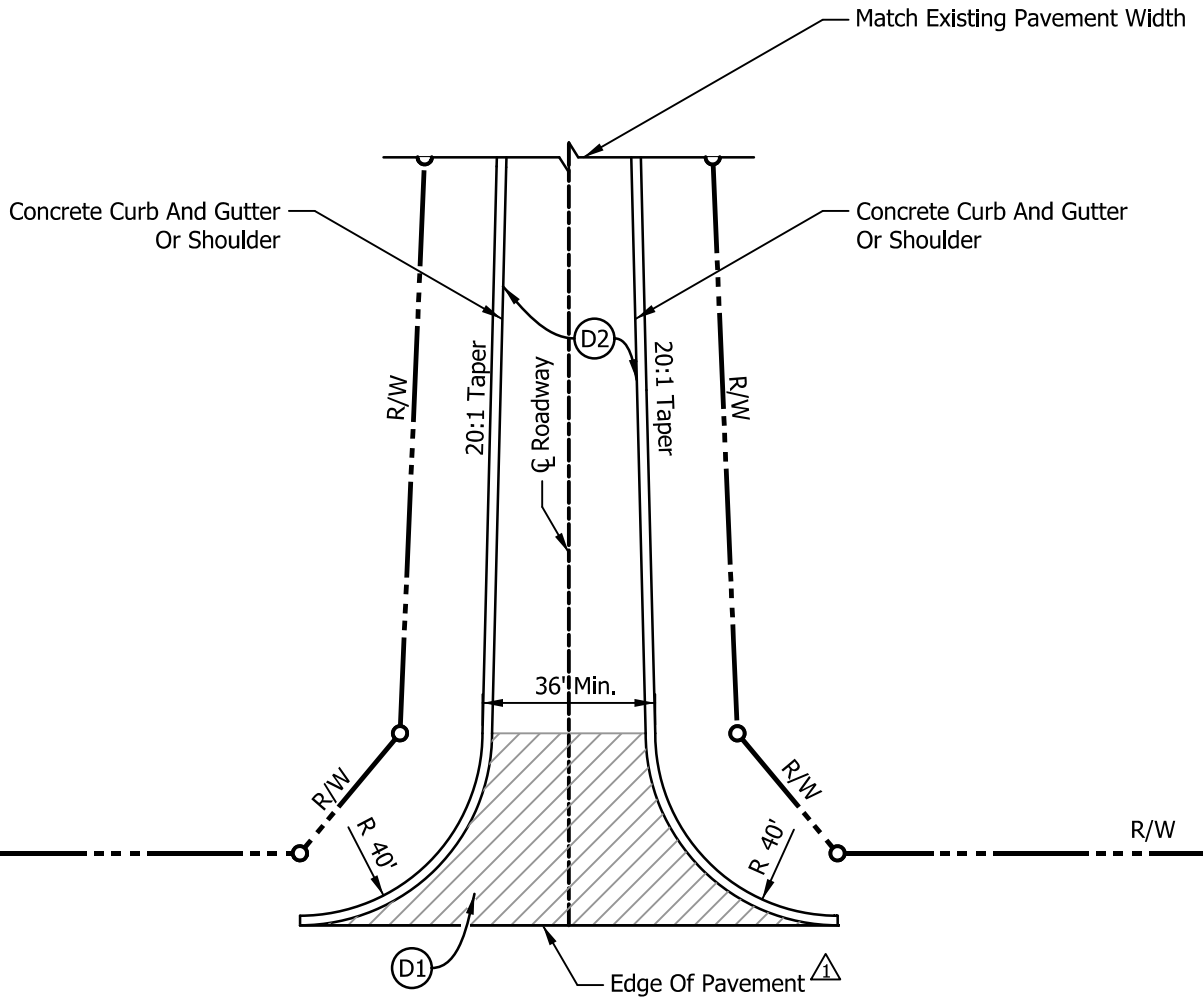
BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL SUBDIVISION ENTRANCE



STANDARD DRAWING
NO. BC-603-006
SHEET 1 OF 1
SCALE: NO SCALE

3/27/2019 7:24:06 AM I:\170004 - Boone County As-Needed Assistance\Standards Consultation\09Road\04 Design files\02 DGN\Plans\6005\603-007_BC_Street_Approach_Sht_Typical.dgn



LEGEND:

- (D1) Match Existing Roadway Pavement Section
- (D2) Match Pavement Section From Residential Feeder STD Drawing BC-400-008

NOTES:

- △ Acceleration/Deceleration Taper To Used To Tie Into Existing, See STD Drawing BC-603-008

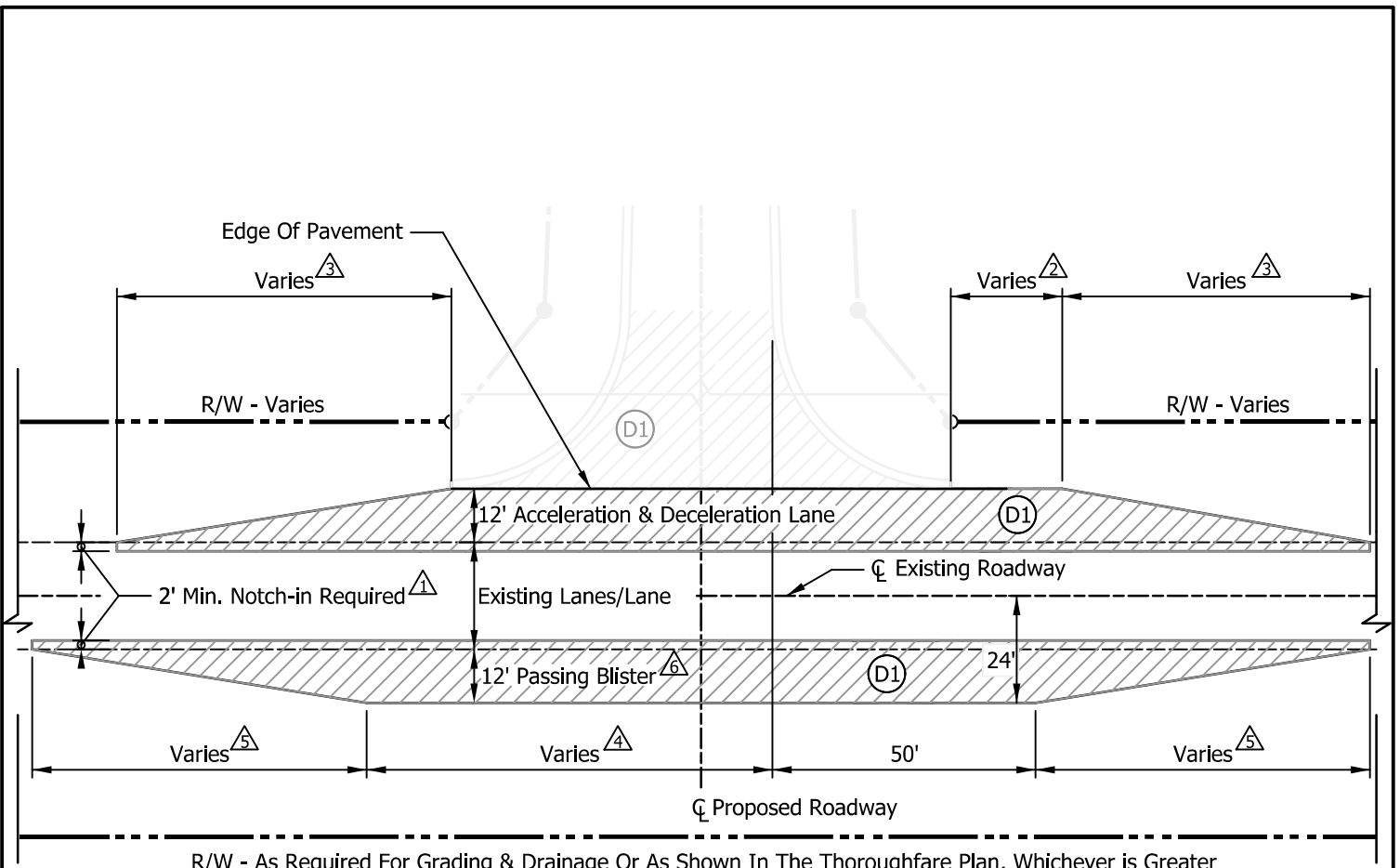
BOONE COUNTY HIGHWAY DEPARTMENT



TYPICAL STREET APPROACH

STANDARD DRAWING
NO. BC-603-007
SHEET 1 OF 1
SCALE: NO SCALE

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R/W - As Required For Grading & Drainage Or As Shown In The Thoroughfare Plan, Whichever is Greater

NOTES:

- △1 Acceleration/Deceleration & Passing Blister To Be Notched Into Existing Pavement With A Minimum Of 2' Clean Saw Cut Edge
- △2 Deceleration Tangent See Table
- △3 Acceleration/Deceleration Taper See Table
- △4 Passing Blister Variable Tangent See Table
- △5 Passing Blister Tapers See Table
- △6 Passing Blisters Required When Deemed Necessary By The County Engineer
- 7. Construction Plans Shall Include Centerline Profile Of The Existing Road Being Intersected By The Proposed Road. The Profile Shall Extend A Minimum Of 500' From The Entrance Centerline In Each Direction.

Accel And Decel Dimension		
Speed (MPH)	△2	△3
30 Or Less	75'	125'
Greater Than 30 & Less Than 50	100'	150'
Greater Than 50	100'	200'

Passing Blister Dimension		
Speed (MPH)	△4	△5
30 Or Less	150'	150'
Greater Than 30 & Less Than 50	150'	200'
Greater Than 50	200'	300'

LEGEND:

- ⊙D1 Match Existing Roadway Pavement Section

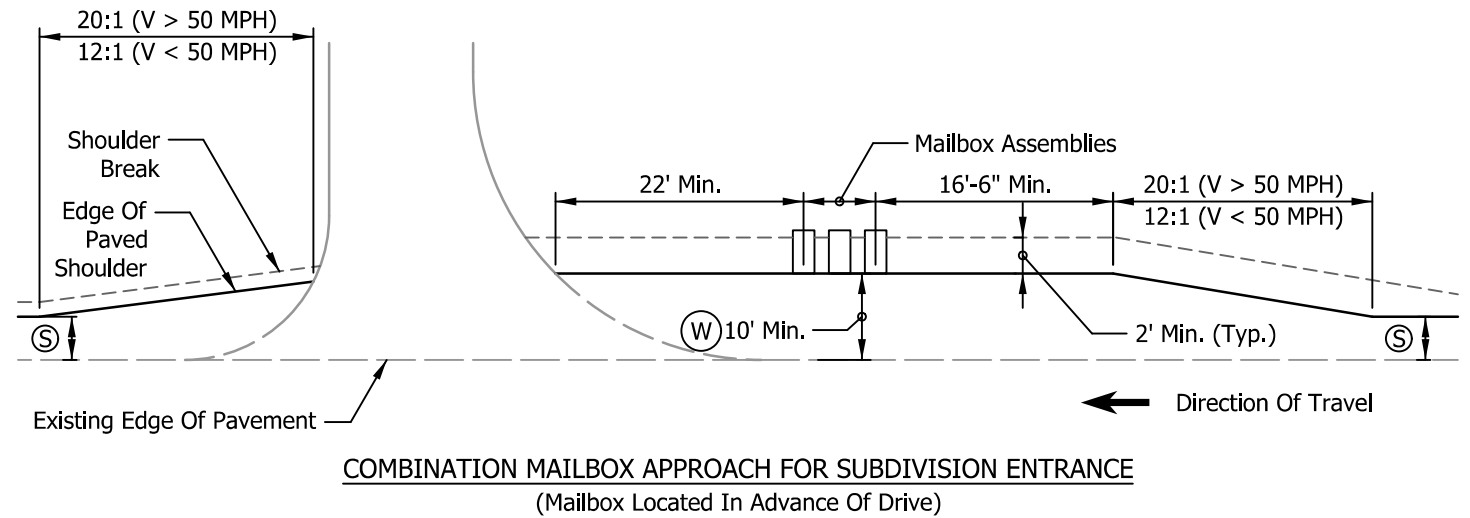
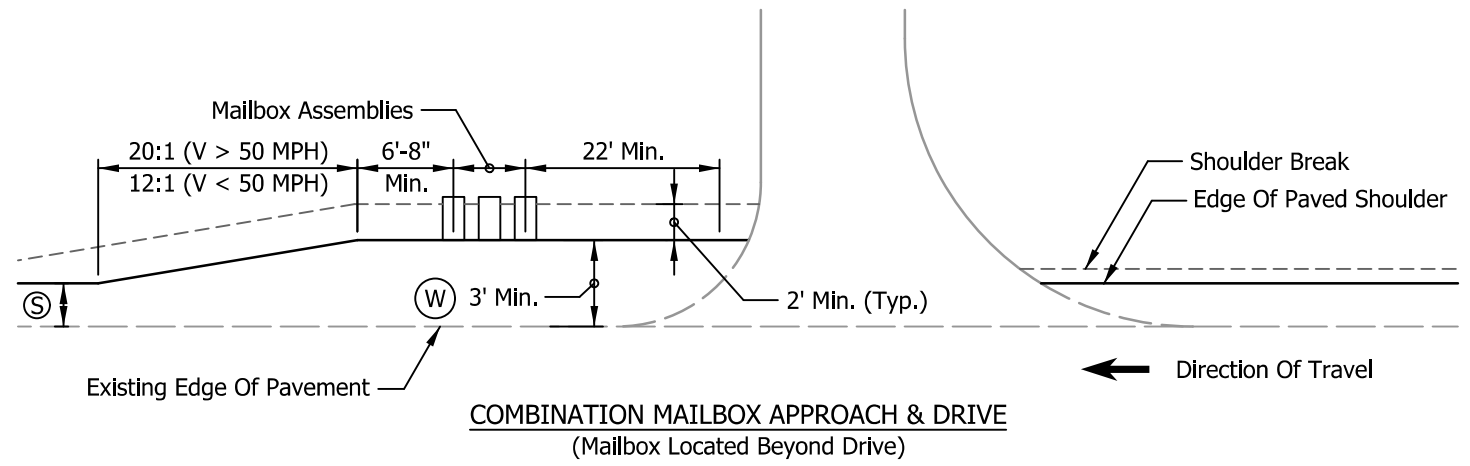
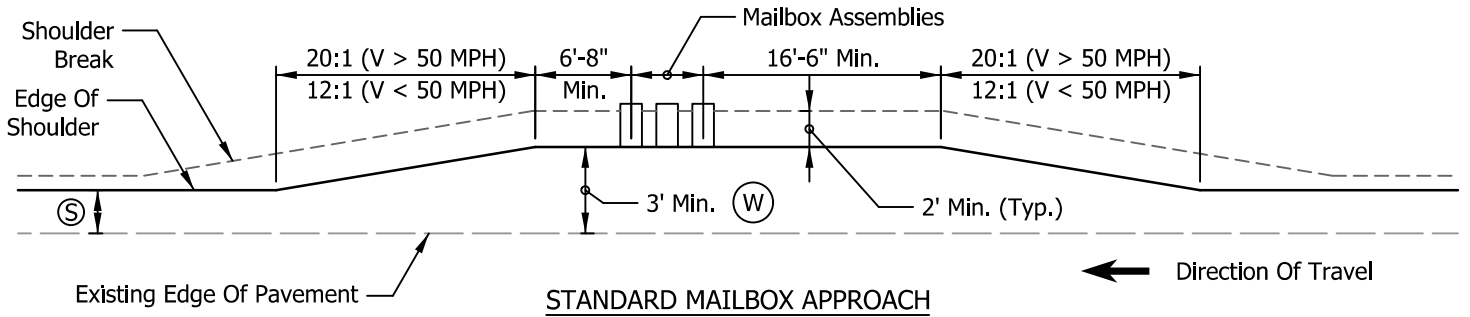
BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL ACCELERATION / DECELERATION LANE

STANDARD DRAWING
NO. BC-603-008
SHEET 1 OF 1
SCALE: NO SCALE



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LEGEND:

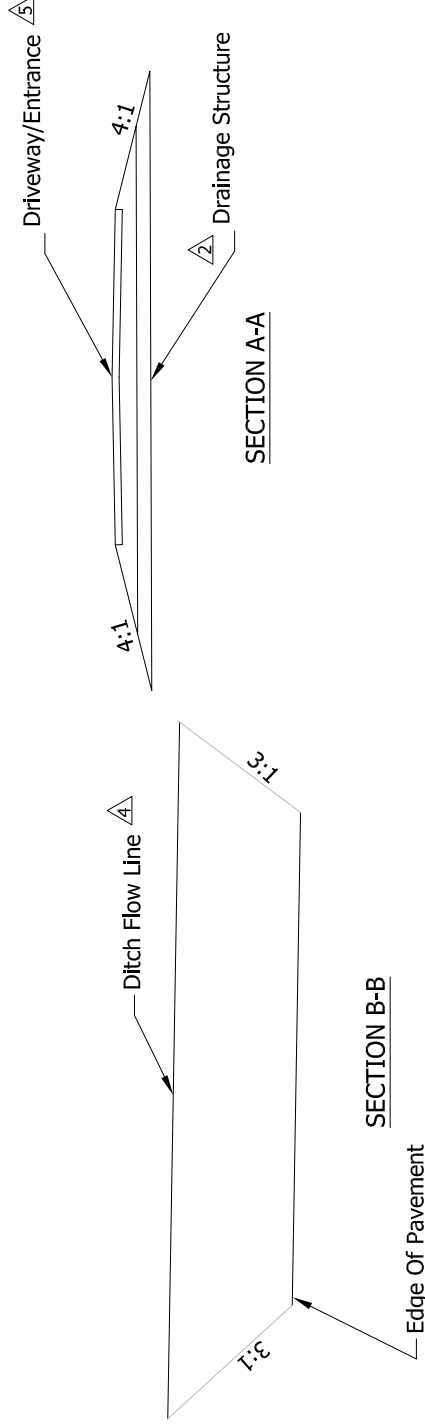
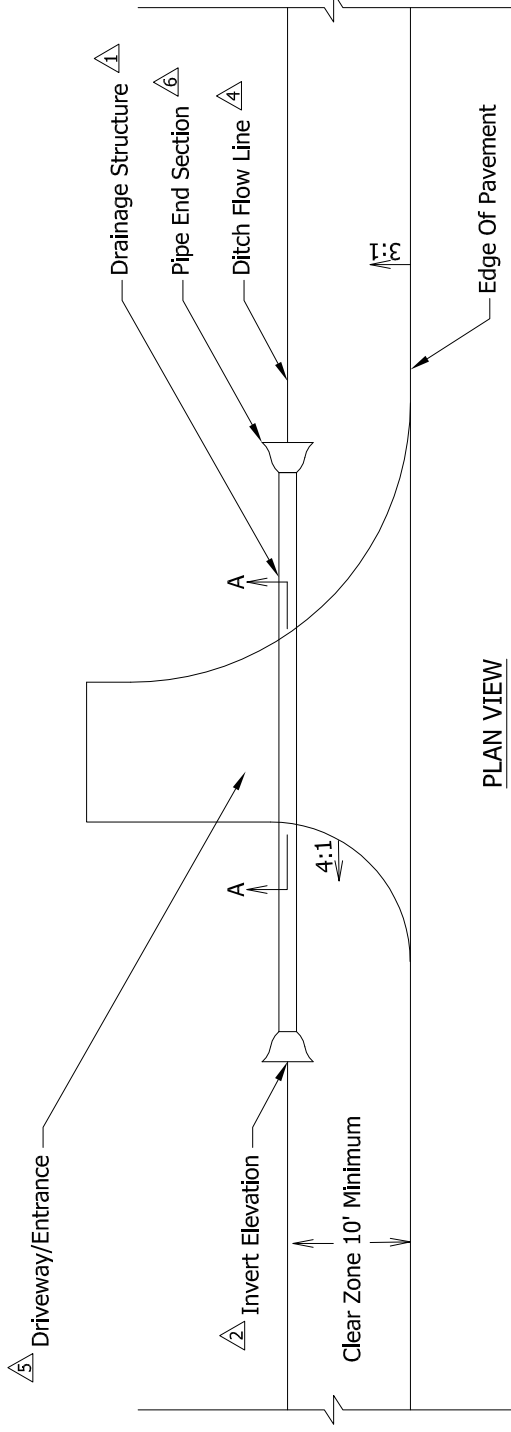
- (W) Match Existing Adjacent Pavement Section (Shoulder Or Lane)
- (S) Shoulder Width From Plans

BOONE COUNTY HIGHWAY DEPARTMENT

TYPICAL MAILBOX APPROACH



STANDARD DRAWING
NO. BC-603-009
SHEET 1 OF 1
SCALE: NO SCALE



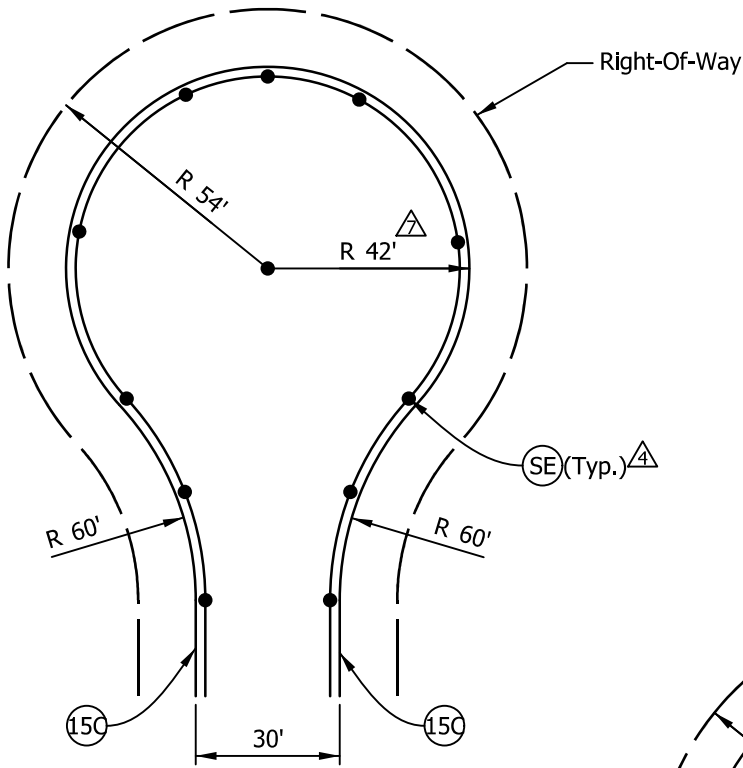
NOTES:

- 1. 12" Min. Diameter Structure (15" Diameter Desirable)
Or As Directed By Boone County Engineer, Minimum 12" Cover
- 2. Invert Elevations Shall Be Provided At All Downstream And Upstream Inverts.
- 3. Any And All Necessary Ditch And Drainage Improvements Shall Be Provided By Individual Performing The Installation
- 4. Flow Line Elevations Shall Be Provided At The Beginning, End, And Every Grade Break.
- 5. For Subdivision Residential Driveway Details - See STD Drawing BC-603-001
For Urban Commercial Driveway Details - See STD Drawing BC-603-002
For Field Entrance Details - See STD Drawing BC-603-003
For Rural Residential Drive - See STD Drawing BC-603-004
- 6. Pipe End Sections Shall Be In Accordance With Clear Zone Criteria Outlined In Current Version Of AASHTO Roadside Design Guide
Drainage Structure Should Be a Min of 10' From Edge Of Pavement

REVISED MAY 2021

BOONE COUNTY HIGHWAY DEPARTMENT	
TYPICAL CULVERT DETAIL WITH OPEN DITCHES	
	STANDARD DRAWING NO. BC-603-010 SHEET 1 OF 1 SCALE: NO SCALE

3/27/2019 7:24:08 AM I:\170004 - Boone County As-Needed Assistance\Standards Consultation\09Road\04 Design files\02 DGN\Plans\6005\604-001_BC_Sub Div Cul-De-Sac_Sht_Typical.dgn



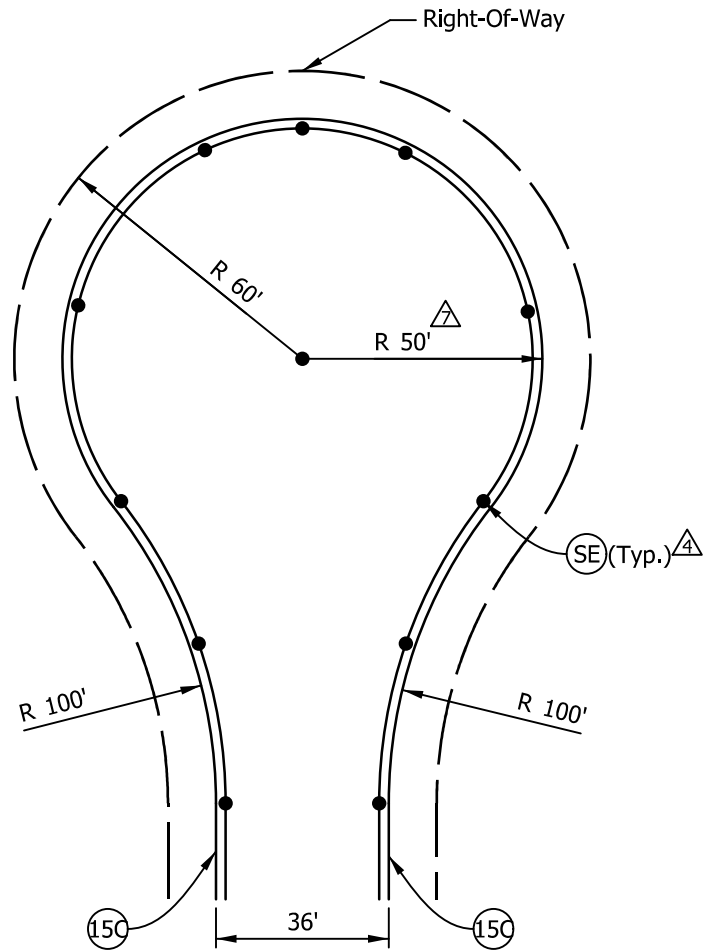
TYPE-1 (CUL-DE-SAC)

NOTES:

1. Type-1 Shall Be Used Only In Residential Subdivisions, All Others Shall Be Type-2.
2. Warning Signs Shall Be Posted At The Street Entrance, Indicating No Outlet.
3. One Detail Shall Be Provided For Each Cul-De-Sac. Each Detail Shall Be Identified By Street Name.
- △ Elevations Provided Shall Be Proposed Flow Line Of Gutter.
5. Street Width And Radius Lengths Are Measured To The Back Of Curb.
6. No Center Islands Permitted.
- △ Radius Shall Be In Accordance With Current Fire Protection Ordinance. May Differ From Dimensions Shown.

LEGEND:

- ⊙ Rolled Curb, See STD Drawing BC-601-001
- ⊙ SE Spot Elevations To Be Provided At Locations As Described Below:
Beginning, Middle, And End Of Curves Approximately 40' Spacing
Along Cul-De-Sac Edge Of Pavement Center Of Cul-De-Sac (High-Point)



TYPE-2 (CUL-DE-SAC)

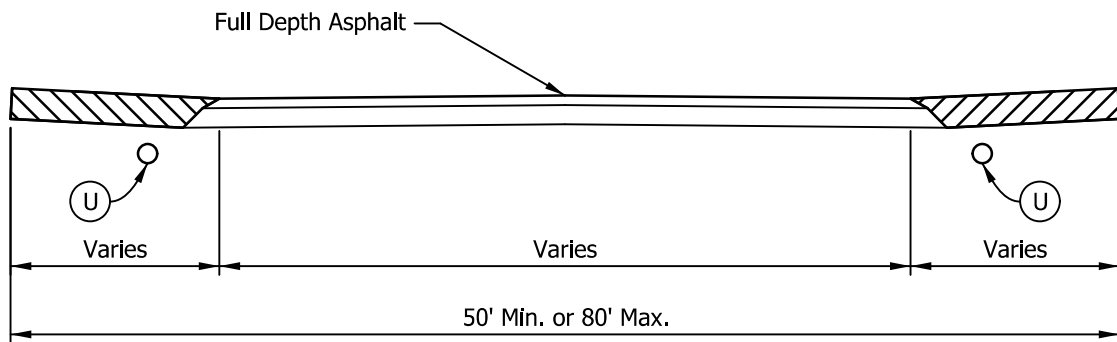
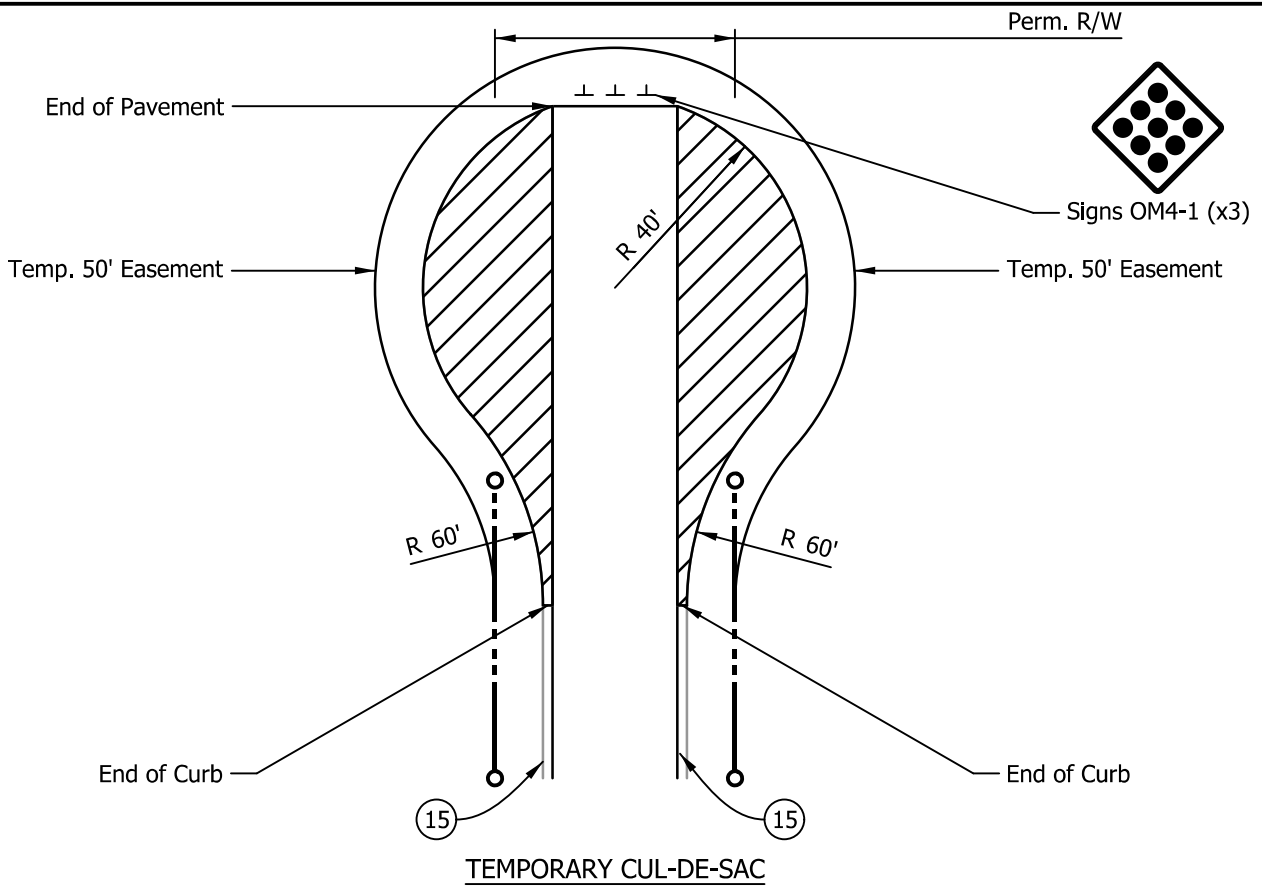
BOONE COUNTY HIGHWAY DEPARTMENT

CUL-DE-SAC



STANDARD DRAWING
NO. BC-604-001
SHEET 1 OF 1
SCALE: NO SCALE

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TEMPORARY CUL-DE-SAC TYPICAL

LEGEND

- ⑮ Concrete Curb And Gutter - See STD Drawing BC-601-001
- Ⓢ Underdrain (Typ.) - See STD Drawing BC-401-001
- Temporary Pavement To Be Removed When Street Is Continued. Thickness And Type To Match Permanent Pavement.

BOONE COUNTY HIGHWAY DEPARTMENT



TEMPORARY CUL-DE-SAC

STANDARD DRAWING
NO. BC-604-002
SHEET 1 OF 1
SCALE: NO SCALE