

SECTION CORNER RECORD

NW 200-400

BOONE COUNTY SURVEYOR'S OFFICE

State Plane grid coordinates NAD83 (2011):
North: 1756068.65 East: 3103638.65 (U.S. survey feet)

Type of monument: Harrison, 4" disk 'Section Corner'
Depth: six inches below adjacent gravel road surface

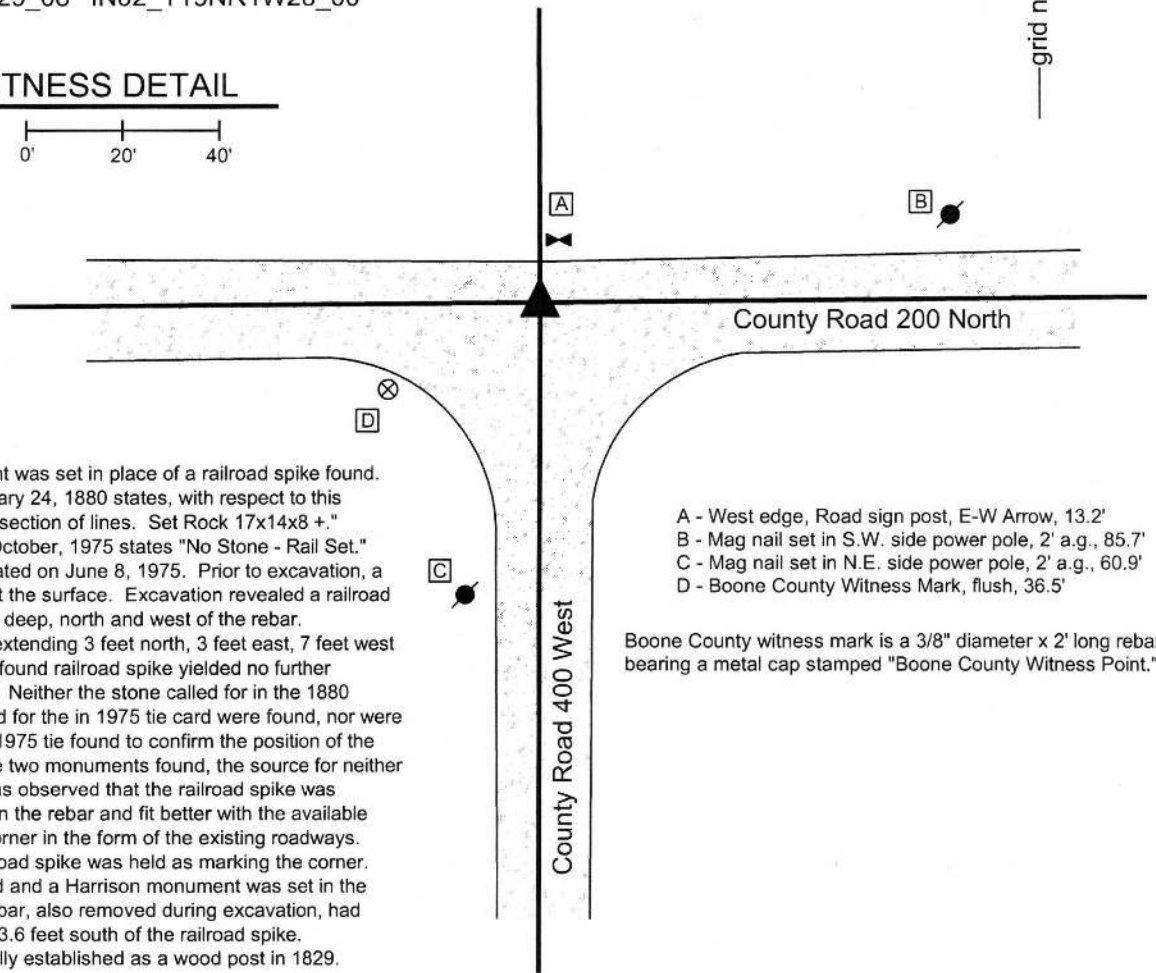
Indiana tie card references:

IN02_T19NR1W20_80 IN02_T19NR1W21_72
IN02_T19NR1W29_08 IN02_T19NR1W28_00



CORNER WITNESS DETAIL

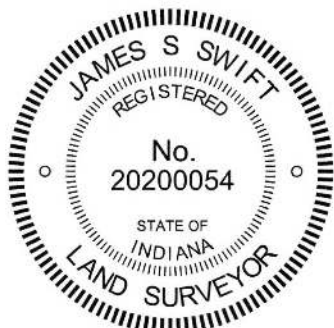
SCALE 1" = 40'



The Harrison monument was set in place of a railroad spike found. A BCS record of February 24, 1880 states, with respect to this corner, "Reestb at intersection of lines. Set Rock 17x14x8 +." A BCS tie card dated October, 1975 states "No Stone - Rail Set." This corner was excavated on June 8, 1975. Prior to excavation, a rebar was found bent at the surface. Excavation revealed a railroad spike found at 8 inches deep, north and west of the rebar. Excavation of an area extending 3 feet north, 3 feet east, 7 feet west and 7 feet south of the found railroad spike yielded no further evidence of the corner. Neither the stone called for in the 1880 record nor the rail called for the in 1975 tie card were found, nor were sufficient ties from the 1975 tie found to confirm the position of the rail. With respect to the two monuments found, the source for neither of which is known, it was observed that the railroad spike was significantly deeper than the rebar and fit better with the available local evidence of the corner in the form of the existing roadways. The position of the railroad spike was held as marking the corner. The spike was removed and a Harrison monument was set in the same position. The rebar, also removed during excavation, had been 1.0 foot east and 3.6 feet south of the railroad spike. This corner was originally established as a wood post in 1829.

- A - West edge, Road sign post, E-W Arrow, 13.2'
- B - Mag nail set in S.W. side power pole, 2' a.g., 85.7'
- C - Mag nail set in N.E. side power pole, 2' a.g., 60.9'
- D - Boone County Witness Mark, flush, 36.5'

Boone County witness mark is a 3/8" diameter x 2' long rebar bearing a metal cap stamped "Boone County Witness Point."



Kenneth E. Hedge, County Surveyor

James S. Swift, IN LS 20200054
March 24, 2016

Jefferson Township	Center Township
Section 20 T19N, R1W	Section 21 T19N, R1W
Section 29 T19N, R1W	Section 28 T19N, R1W

SECTION CORNER RECORD

NW 200-450

BOONE COUNTY SURVEYOR'S OFFICE

State Plane grid coordinates NAD83 (2011):

North: 1756048.33 East: 3100985.010 (U.S. survey feet)

Type of monument: Harrison, 4" disk 'Section Corner'

Depth: six inches below adjacent gravel road surface

Indiana tie card references:

IN02_T19NR1W20_72

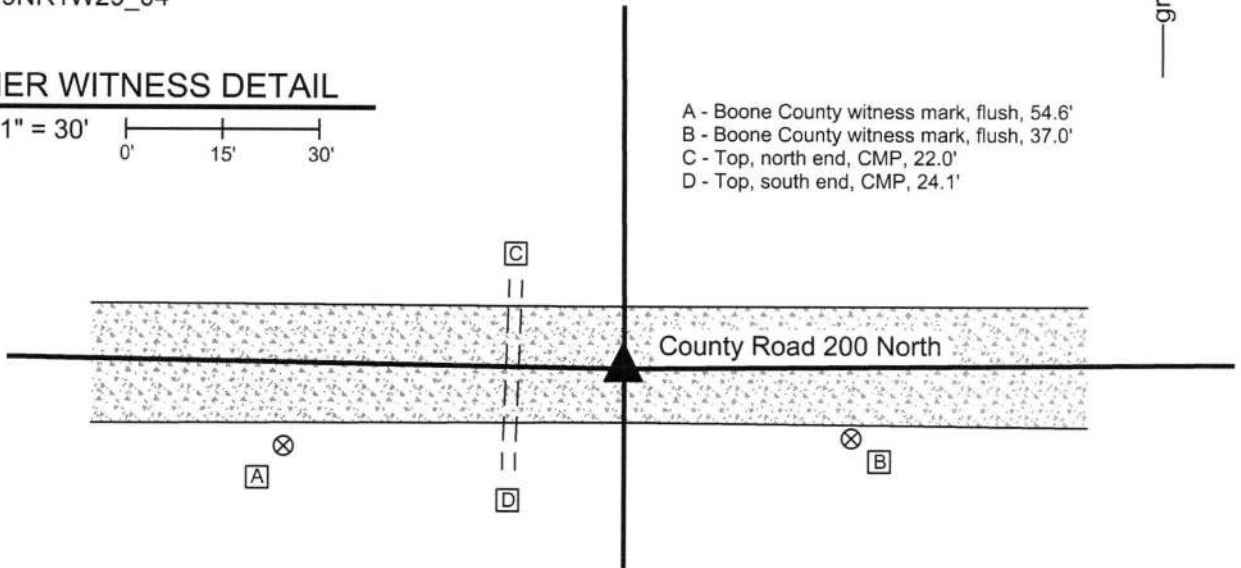
IN02_T19NR1W29_04



CORNER WITNESS DETAIL

SCALE 1" = 30'

- A - Boone County witness mark, flush, 54.6'
- B - Boone County witness mark, flush, 37.0'
- C - Top, north end, CMP, 22.0'
- D - Top, south end, CMP, 24.1'



The Harrison monument was set in place of a vertical section of 1/2 inch diameter rebar found.

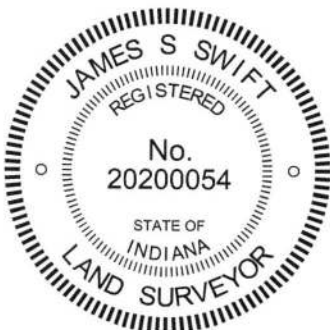
No specific descriptions of this corner were found in the BCS Surveyor's Record, though the corner is noted as the terminal point for measurements in several entries, most notably in an entry from November, 1853 which shows the relative locations of the subject corner and the section corners to the east and west. No recent BCS tie cards were found referencing this corner.

Excavation was performed at this corner on November 19, 2015. Prior to excavation, two rebars were found in the general vicinity of the corner. A 5/8 inch diameter rebar with a red cap, markings illegible, was found at 1 inch deep in the gravel road. A 1/2 inch diameter rebar with no cap was found at 9 inches deep in the gravel road, approximately 1 foot south and 3 feet west of the shallower rebar. Both rebars were in the approximate center of the east-west roadway. The 1/2 inch diameter rebar was at the west edge of a vague field entrance known to have formerly been used to access the field to the south and east, per testimony of local landowners.

Excavation of an area extending 4 feet in all directions from the 1/2 inch diameter rebar yielded no further evidence of the corner. Numerous fist-sized rocks were found at the base of the road bed. None of these stones appeared to be section corner monuments and the presence of this substantial base material indicates that this specific area was formerly wet and muddy.

It is noted that the roadway bends significantly at this quarter corner. Contrary to the expectation that the east-west section line be generally straight, the road diverges from the straight line. The center of the road and existing monuments were found to be up to 30 feet south of a straight line connecting the section corners to the east and west. The 1939 aerial photos on file in this office indicate that the bend in the road has existed since at least that time. Of the two monuments, the 1/2 inch rebar fit better with the local evidence of the corner in the form of the old farm lane, fit better with remote evidence of the corner with respect to evidence of occupation along the sixteenth line 1/4 mile to the west and fit better with the measurements reported in the 1853 record. The 5/8 inch rebar appears to have been set at or approximately equidistant from previously accepted (though not currently accepted) positions of the section corners to the east and west. However, the 1853 record indicates that the distance between the quarter corner (subject corner) and the corner to the west is longer than the distance to the quarter corner to the east. On all counts, the deeper monument fit better with old evidence and BCS records than the shallower monument. The 1/2 inch rebar was held as marking the corner and was replaced with a Harrison monument in the same position. Both rebars were removed during excavation. The 5/8 inch rebar had been 0.9 foot north and 3.1 feet east of the corner.

This corner was originally established as a wood post in 1822.



Kenneth E. Hedge, County Surveyor

James S. Swift, IN LS 20200054
March 23, 2017

Jefferson
Township

Section 20
T19N, R1W

Section 29
T19N, R1W

SECTION CORNER RECORD

NW 200-500

BOONE COUNTY SURVEYOR'S OFFICE

State Plane grid coordinates NAD83 (2011):

North: 1756092.46 East: 3098338.74 (U.S. survey feet)

Type of monument: Harrison, 4" disk 'Section Corner'

Depth: flush with adjacent road surface

Indiana tie card references:

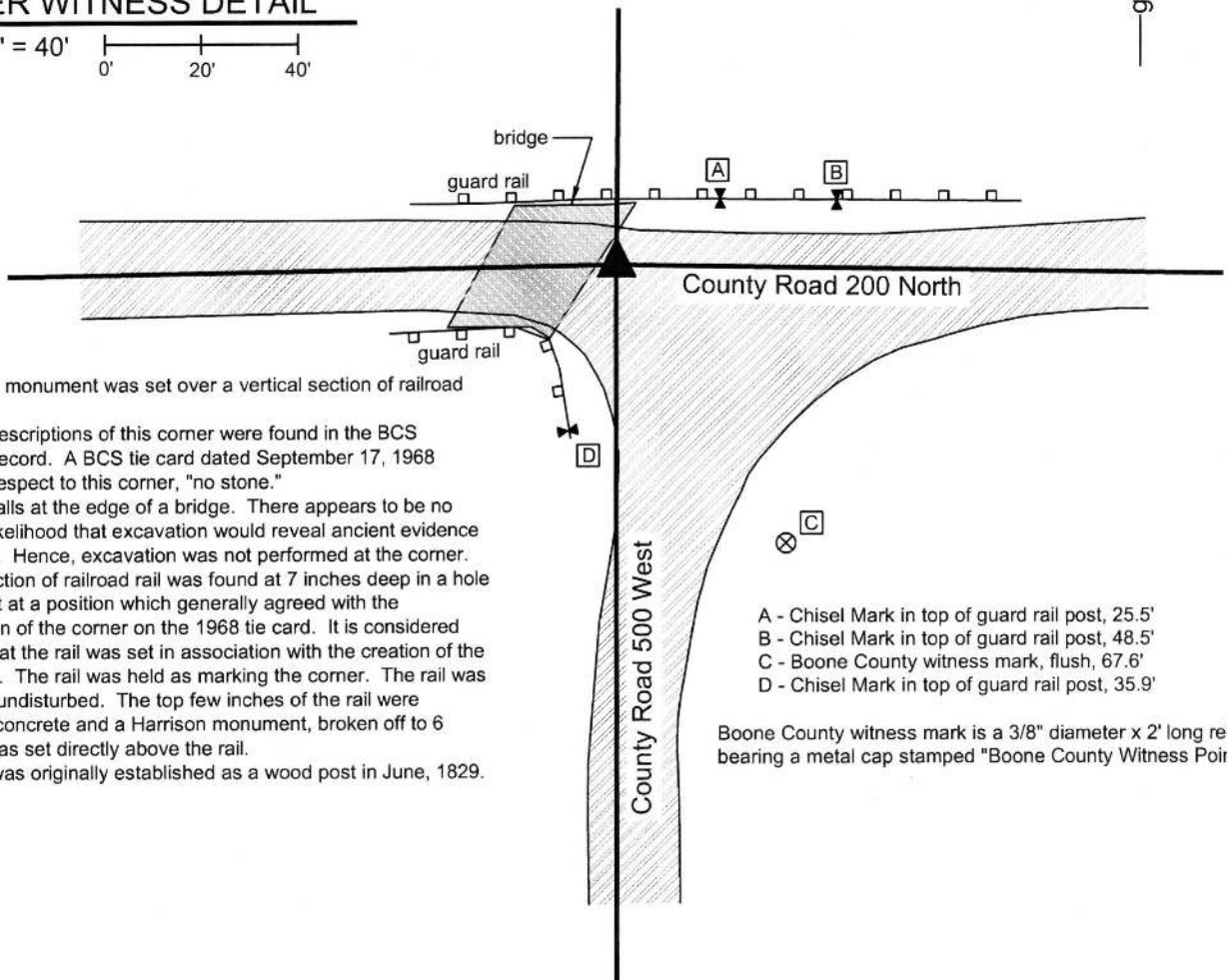
IN02_T19NR1W19_80 IN02_T19NR1W20_72

IN02_T19NR1W30_08 IN02_T19NR1W30_00



CORNER WITNESS DETAIL

SCALE 1" = 40'



The Harrison monument was set over a vertical section of railroad rail found.

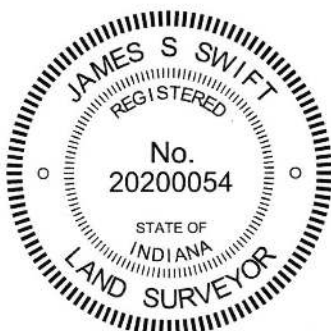
No specific descriptions of this corner were found in the BCS Surveyor's Record. A BCS tie card dated September 17, 1968 states, with respect to this corner, "no stone."

This corner falls at the edge of a bridge. There appears to be no substantial likelihood that excavation would reveal ancient evidence of the corner. Hence, excavation was not performed at the corner. A vertical section of railroad rail was found at 7 inches deep in a hole in the asphalt at a position which generally agreed with the representation of the corner on the 1968 tie card. It is considered most likely that the rail was set in association with the creation of the 1968 tie card. The rail was held as marking the corner. The rail was left in place, undisturbed. The top few inches of the rail were anchored in concrete and a Harrison monument, broken off to 6 inches tall, was set directly above the rail.

This corner was originally established as a wood post in June, 1829.

- A - Chisel Mark in top of guard rail post, 25.5'
- B - Chisel Mark in top of guard rail post, 48.5'
- C - Boone County witness mark, flush, 67.6'
- D - Chisel Mark in top of guard rail post, 35.9'

Boone County witness mark is a 3/8" diameter x 2' long rebar bearing a metal cap stamped "Boone County Witness Point."



Kenneth E. Hedge
Kenneth E. Hedge, County Surveyor

James S. Swift
James S. Swift, IN LS 20200054
March 24, 2016

Jefferson Township

Section 19 T19N, R1W	Section 20 T19N, R1W
Section 30 T19N, R1W	Section 29 T19N, R1W

SECTION CORNER RECORD

NW 250-500

BOONE COUNTY SURVEYOR'S OFFICE

State Plane grid coordinates NAD83 (2011):

North: 1758736.08 East: 3098365.29 (U.S. survey feet)

Type of monument: Harrison, 4" disk 'Section Corner'

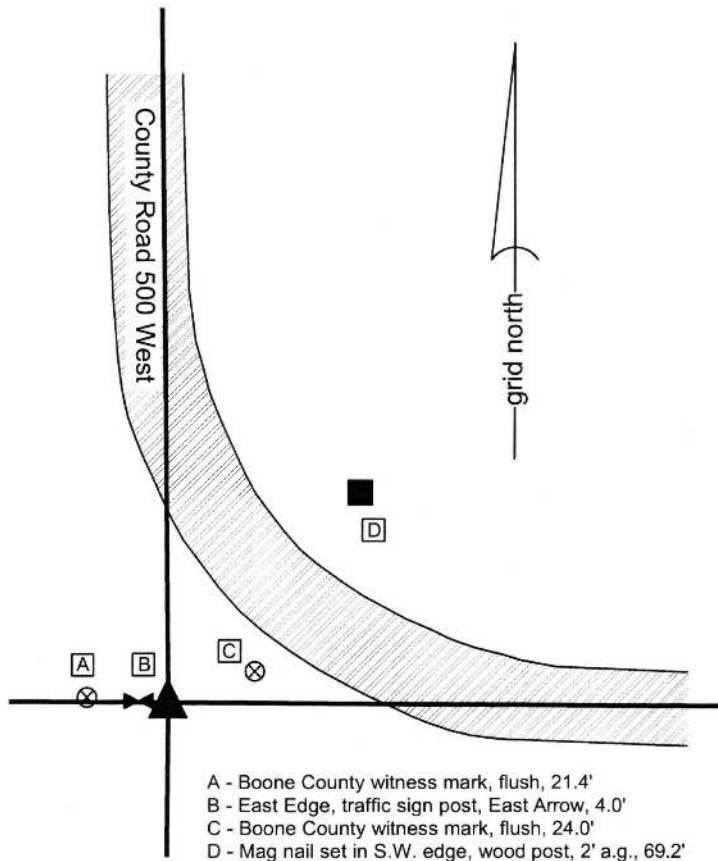
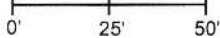
Depth: Flush with adjacent ground

Indiana tie card references:

IN02_T19NR1W19_44 IN02_T19NR1W20_36

CORNER WITNESS DETAIL

SCALE 1" = 50'



Boone County witness mark is a 3/8" diameter x 2' long rebar bearing a metal cap stamped "Boone County Witness Point."

The Harrison monument was set in place of a rebar found. No specific descriptions of this corner were found in the BCS Surveyor's Record, though the corner is mentioned as the terminal point for measurements in records of 1839 and 1893. A BCS tie card dated September 17, 1968 shows the corner as a "stone located." This document contains a drawing which graphically demonstrates the corner to be at the center of a 90° degree bend in the roadway. The corner is clearly represented as being within the physical roadway. The actual situation is that the corner is west of the roadway. Though the road does achieve a 90° bend, the roadway itself does not extend as far west as the corner. The current position of the road appears to be substantially similar to the position of the road as shown on aerial photos as early as 1939. It is evident, though, that the current roadway has been built up in elevation relative to the surrounding ground to the west. This is noted, because the 1968 tie card call for "stone located" indicates that a stone should be present or perhaps a vertical section of railroad rail. It is known that BCS field crews from the late 1960s and early 1970s followed a common practice of removing a stone, if found, and replacing it with a railroad rail. Neither a stone nor a railroad rail were found at the corner. In some cases, these crews appear to have replaced the stone with a rebar, rather than a rail, but this practice was not common until the mid-1970s.

No reference ties are known to exist from the 1968 tie card. One power pole to the west may be the same as is shown the the card but it is not clear that this is the same pole and the graphical representation of the corner of the tie card shows the power pole to be southwest of the corner.

A rebar was found at a probable location for the corner. The rebar was flush with the surface of the dirt but appeared to be leaning towards the northwest. Extensive probing and minor excavation revealed no other evidence at the corner. Local evidence of the corner does exist in the form of County Road 500 West to the north of the corner and an existing fence to the west. Establishing a theoretical position for the corner based on prolonging the line defined by the fence toward the east and prolonging the line defined by the center of the road to the south would place the corner approximately 2 feet north of the rebar. Establishing a theoretical position for the corner based on adjoining deed descriptions to the south and monumentation found marking corners of the described parcels places the corner slightly north, but within one-half foot, of the rebar as found, and as stood vertical. Excavation around the rebar revealed substantial rust in the dirt adjacent to the monument which indicates that the monument had been present in that position for a long period of time.

Given the lack of contrary evidence, the harmony of the position of the rebar with adjacent deed descriptions and monumentation, and the evident long duration of time in which the rebar was at this position, the rebar was held as the best available evidence of the corner. It is also noted that the rebar, as stood vertically, fit within 0.5 foot of the power pole which may, or may not, have been a reference tie on the 1968 tie card. The rebar was removed and replaced with a Harrison monument in the same position. The actual position of the monument is coincident with the position of the bottom of the rebar as originally found, given that the rebar appeared to be leaning slightly northwest. Two possible explanations are offered for the fact that neither a stone nor a rail were found at the corners. The first is that the BCS crews may have set the rebar, though the more common practice at the time was to set a railroad rail. The other possible explanation is that the adjacent road bed was improved and built up on fill material some time after 1968, which earth-moving activity would have likely disturbed the position of the corner and any monument which may have existed there at the time.

This corner was originally established as a wood post in 1829.



Kenneth E. Hedge
 Kenneth E. Hedge, County Surveyor

James S. Swift
 James S. Swift, IN LS 20200054

March 24, 2016

Jefferson Township

Section 19 T19N, R1W	Section 20 T19N, R1W
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SECTION CORNER RECORD

NW 300-400

BOONE COUNTY SURVEYOR'S OFFICE

State Plane grid coordinates NAD83 (2011):

North: 1761322.07 East: 3103613.54 (U.S. survey feet)

Type of monument: Harrison, 4" disk 'Section Corner'

Depth: One inch below adjacent road surface

Indiana tie card references:

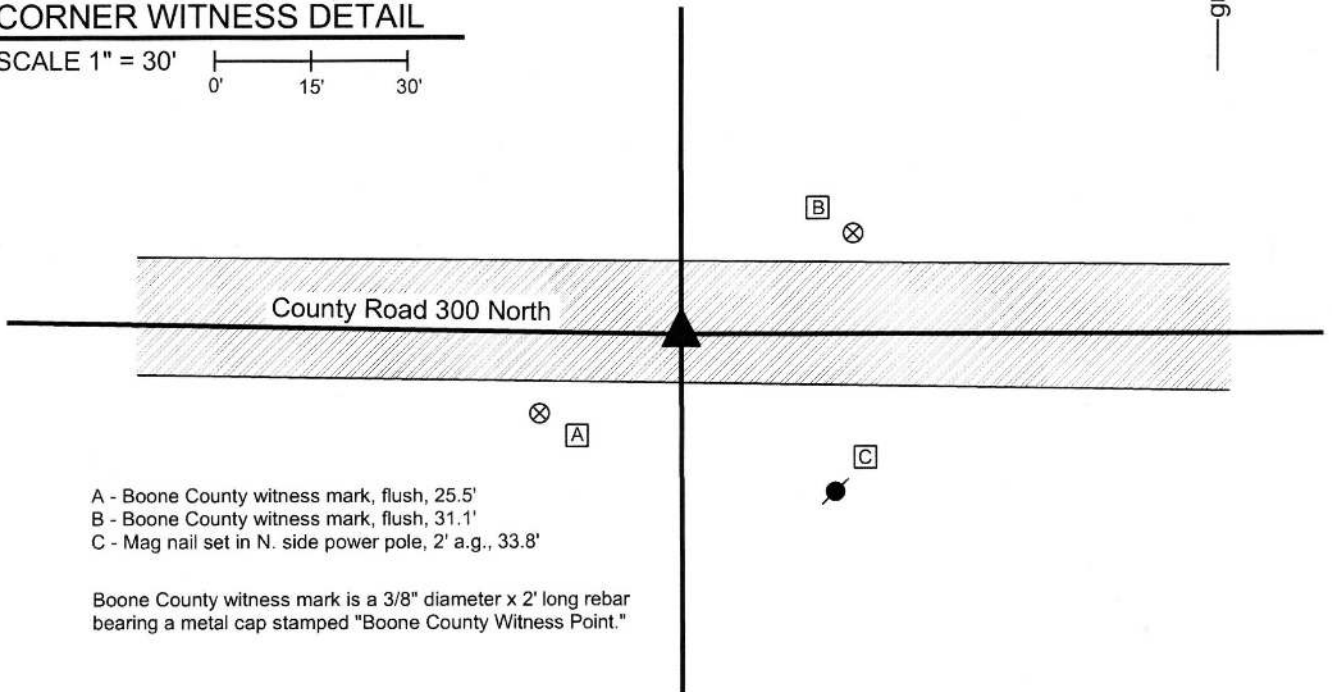
IN02_T19NR1W17_80 IN02_T19NR1W16_72

IN02_T19NR1W20_08 IN02_T19NR1W21_00



CORNER WITNESS DETAIL

SCALE 1" = 30'



- A - Boone County witness mark, flush, 25.5'
- B - Boone County witness mark, flush, 31.1'
- C - Mag nail set in N. side power pole, 2' a.g., 33.8'

Boone County witness mark is a 3/8" diameter x 2' long rebar bearing a metal cap stamped "Boone County Witness Point."

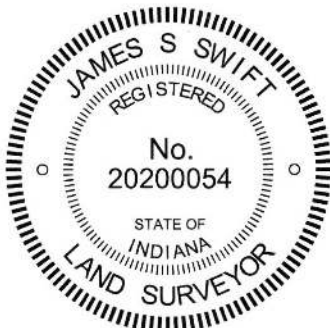
The Harrison monument was set over a vertical section of railroad rail found.

A BCS record of April 8, 1878 states, with respect to this corner, "Set Rock." A BCS tie card dated June 3, 1971 states "Stone located. Replaced stone with RRR."

This corner was excavated on July 2, 2015. Prior to excavation, a railroad spike was found at 3 inches deep in a hole in the asphalt roadway.

Excavation revealed a vertical section of railroad rail found at 13 inches deep. The rail was found to be 0.4 foot west and 0.1 foot north of the railroad spike. Substantial further excavation was not performed, given the statement on the 1971 tie card that the found stone was replaced with a railroad rail. The rail was left in place. The top of the rail was encased in concrete and a Harrison monument was set directly over the rail.

This corner was originally established as a wood post in 1819.



Kenneth E. Hedge, County Surveyor

James S. Swift, IN LS 20200054
March 24, 2016

Washington Township

Section 17 T19N, R1W	Section 16 T19N, R1W
Section 20 T19N, R1W	Section 21 T19N, R1W

Jefferson
Township

Center
Township

SECTION CORNER RECORD

NW 300-450

BOONE COUNTY SURVEYOR'S OFFICE

State Plane grid coordinates NAD83 (2011):

North: 1761375.64 East: 3100965.67 (U.S. survey feet)

Type of monument: Harrison, 4" disk 'Section Corner'

Depth: One inch below adjacent road surface

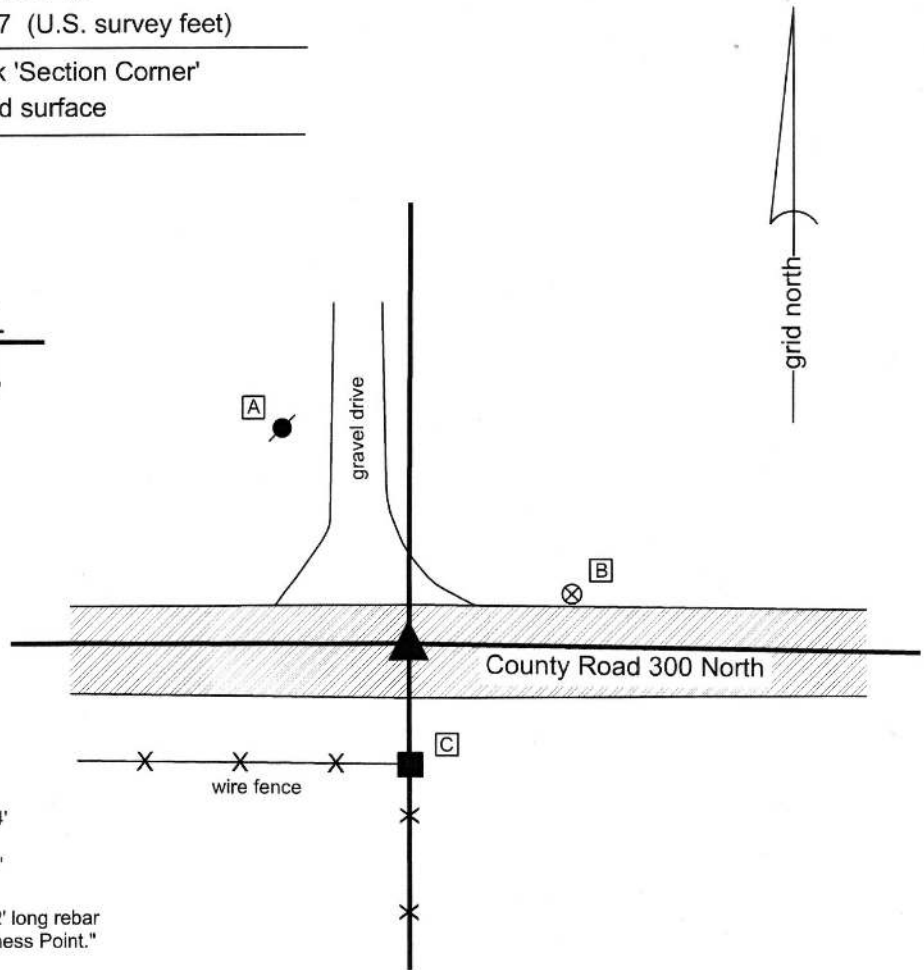
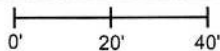
Indiana tie card references:

IN02_T19NR1W17_76

IN02_T19NR1W20_04

CORNER WITNESS DETAIL

SCALE 1" = 40'



- A - Mag nail set in S. side power pole, 2' a.g., 50.4'
- B - Boone County witness mark, flush, 35.3'
- C - Mag nail set in N. side wood post, 2' a.g., 25.0'

Boone County witness mark is a 3/8" diameter x 2' long rebar bearing a metal cap stamped "Boone County Witness Point."

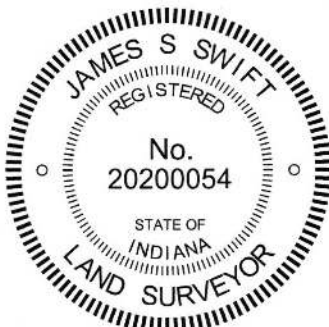
The Harrison monument was set over a stone found.

No specific descriptions of this corner were found in the BCS Surveyor's Record. A BCS tie card dated June 1, 1971 states "Stone Located. RRR slanted over stone."

This corner was excavated on July 1, 2015. Prior to excavation, a railroad spike was found at the surface of the road. Excavation revealed another railroad spike found at 4 inches deep, slightly N.E. of the upper spike. Further excavation revealed a section of railroad rail found at 11 inches deep. The rail was clearly leaning towards the south. The first found railroad spike had been located approximately over the highest point of the leaning rail, being at the south edge of the rail. Two rebars were also found adjacent to the rail. Both were situated horizontally, as found, on the E & W sides of the rail, respectively. The rail and rebars were removed and a stone was found at 18 inches deep. The stone had a distinct, sharp, high point which was 0.2 foot west and 0.1 foot south of the first found spike and the high point of the rail. The top 6 inches of the stone were exposed. At 6 inches below the top, the stone measured 7 inches E-W by 5 inches N-S and appeared that it was likely much larger in all dimensions. The distinct, sharp high point of the stone was at the east end of a distinct ridge across the top of the stone.

The high point of the stone was held as the exact point of the corner and a Harrison monument was set over this point. The rail and both railroad spikes were removed during excavation.

This corner was originally established as a wood post in 1829.



Kenneth E. Hedge
Kenneth E. Hedge, County Surveyor

James S. Swift
James S. Swift, IN LS 20200054
March 24, 2016

Washington Township

Section 17
T19N, R1W

Jefferson Township

Section 20
T19N, R1W

PHOTOGRAPHIC DOCUMENTATION

Date of photography: July 1, 2015

The section of leaning railroad rail, exposed with the high point painted pink.



The stone as it was originally exposed.



The stone from above.



The stone viewed from the south.



View from above the stone looking south along the fence line south of County Road 300 North.



View from above the stone looking west along County Road 300 North.



For details and documentation, refer to related sheet "Section Corner Record."

SECTION CORNER RECORD

NW 300-500

BOONE COUNTY SURVEYOR'S OFFICE

State Plane grid coordinates NAD83 (2011):

North: 1761383.99 East: 3098337.82 (U.S. survey feet)

Type of monument: Harrison, 4" disk 'Section Corner'

Depth: One inch below adjacent road surface

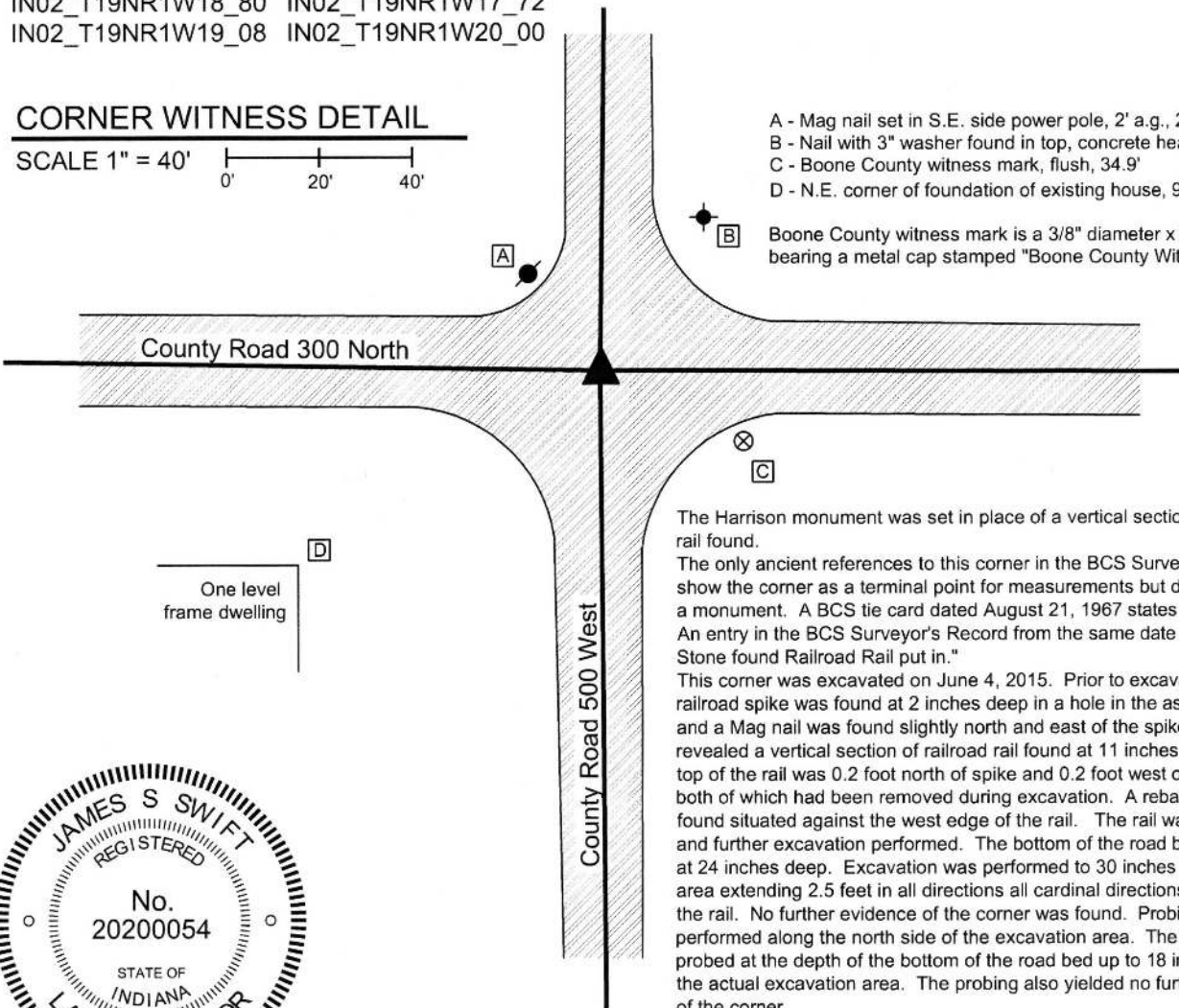
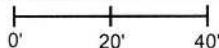
Indiana tie card references:

IN02_T19NR1W18_80 IN02_T19NR1W17_72
IN02_T19NR1W19_08 IN02_T19NR1W20_00



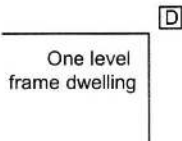
CORNER WITNESS DETAIL

SCALE 1" = 40'



- A - Mag nail set in S.E. side power pole, 2' a.g., 24.4'
- B - Nail with 3" washer found in top, concrete headwall, 40.0'
- C - Boone County witness mark, flush, 34.9'
- D - N.E. corner of foundation of existing house, 92.9'

Boone County witness mark is a 3/8" diameter x 2' long rebar bearing a metal cap stamped "Boone County Witness Point."



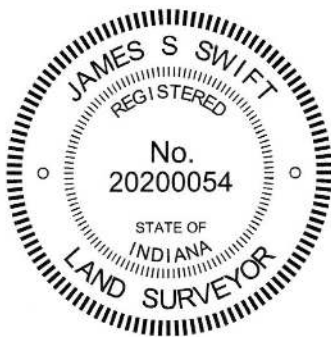
The Harrison monument was set in place of a vertical section of railroad rail found.

The only ancient references to this corner in the BCS Surveyor's Record show the corner as a terminal point for measurements but do not describe a monument. A BCS tie card dated August 21, 1967 states "No stone." An entry in the BCS Surveyor's Record from the same date states "No Stone found Railroad Rail put in."

This corner was excavated on June 4, 2015. Prior to excavation, a railroad spike was found at 2 inches deep in a hole in the asphalt roadway and a Mag nail was found slightly north and east of the spike. Excavation revealed a vertical section of railroad rail found at 11 inches deep. The top of the rail was 0.2 foot north of spike and 0.2 foot west of the mag nail, both of which had been removed during excavation. A rebar was also found situated against the west edge of the rail. The rail was removed and further excavation performed. The bottom of the road bed was found at 24 inches deep. Excavation was performed to 30 inches deep in an area extending 2.5 feet in all directions all cardinal directions, centered on the rail. No further evidence of the corner was found. Probing was performed along the north side of the excavation area. The ground was probed at the depth of the bottom of the road bed up to 18 inches north of the actual excavation area. The probing also yielded no further evidence of the corner.

The railroad rail fit with local evidence of the corner in the form of the approximate centers of the roads which extended in all cardinal directions from the corner. The rail was held as marking the corner. The rail was removed during excavation and a Harrison monument was set in the same position as the rail.

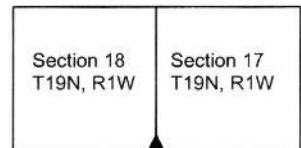
This corner was originally established as a wood post in 1829.



Kenneth E. Hedge
Kenneth E. Hedge, County Surveyor

James S. Swift
James S. Swift, IN LS 20200054
March 24, 2016

Washington Township



Jefferson Township

